

NAVY BLUE

A Floating Community ■ Charlestown Marina

JANUARY 6, 2021

Response to Request for Proposals for
PIER 5 Charlestown Navy Yard
Boston, Massachusetts

SUBMITTED BY:

Navy Blue, LLC

a Tri-Venture of

Charlestown Marina / Parent+Diamond / Urban Spaces



January 6, 2021

Ms. Teresa Polhemus
Executive Director/Secretary
Boston Planning & Development Agency
Municipal Protective Service Desk, First Floor
12 Channel Street
South Boston, MA 02210

Dear Executive Director/Secretary Polhemus:

It is with great pleasure that we present NAVY BLUE, a redevelopment concept for Pier 5 in the Charlestown Navy Yard. NAVY BLUE will be the most effective, dynamic, and successful long-term approach for Pier 5 because it:

- Ignites energy and excitement at the waterfront by expanding the Charlestown Marina to establish a contemporary and unique floating community.
- Embraces cutting edge approaches to ensure environmental sensitivity and resiliency.
- Revitalizes the Harbor Walk and restores the historic Pump House to create a distinctive public amenity.
- Launches an economic development engine that provides opportunities for jobs and wealth creation for minority, women, and veteran-owned businesses and individuals who have historically been underrepresented in real estate development.
- Engages collaboratively with Courageous Sailing to bolster their mission and ongoing operations.

NAVY BLUE is grounded in Charlestown Marina's seven-year history of working in the Navy Yard to transform the adjacent Pier 6 and Pier 8 into best-in-class marinas. Our proposed development innovates beyond the traditional marina to create a one-of-a-kind community experience with a sleek and sophisticated "aquatectural" aesthetic.

We intend to raze and recycle the existing pier and replace it with generously-proportioned marina walkways that feature overhead solar canopies leading to 55 slips. The slips will be fitted with "green" live-aboard vessels (LVs) with independent living quarters ranging in size from 530 to 2,100 square feet. In the same way that the Navy Yard has evolved from its original military purpose to become one of Boston's most vibrant communities, NAVY BLUE will mirror this spirit of transition as we participate in the adaptation along the waterfront from industrial to other uses. Furthermore, our program supports the Climate Ready Boston initiative by planning for the impacts of climate change and building a program with a resilient future.

NAVY BLUE will be a year-round marina community and will feature a floating pavilion, which will serve as the marina's communal amenity space as well as a water taxi stop. To facilitate marina operations, such as bike storage, mail and package delivery, and other back-of-house functions, we've entered into discussions

with Flagship Wharf to secure access to nearby first floor commercial space, as well as garage parking spaces, which will eliminate the need for on-site parking.

We will extend Charlestown Marina's ongoing renovation of the Harbor Walk from Pier 8 to Pier 6 by spearheading the revitalization of the stretch of Harbor Walk from Pier 6 to the historic Building 123 Pump House, which we will preserve and adapt for reuse as a community gathering space for Charlestown residents, the Friends of Charlestown Navy Yard, and Courageous Sailing ("CS"). In addition, NAVY BLUE and CS are collaborating to explore an array of ideas that would help facilitate CS's mission, including sliding Pier 6's property line to the east to enable an expansion of CS's water sheet. (Given Charlestown Marina's ownership of Pier 6, NAVY BLUE is the only group that can provide this support.)

Our approach to development execution is as innovative as our floating community concept. With an MBE tri-venture partner, NAVY BLUE is establishing a development platform to ensure diversity, equity, and inclusion. In addition, we're providing a mechanism for minority, women, and veteran-owned businesses and individuals to make micro-investments that will promote broader participation in the project's ultimate success. Our commitment extends to every stage of the development cycle, including ownership, pre-development, construction, and ongoing daily marina operations.

Best regards,

A handwritten signature in blue ink, reading "Charles Lagasse, Jr." in a cursive script.

Charles Lagasse, Jr.

Navy Blue, LLC

A tri-venture of Charlestown Marina / Parent+Diamond / Urban Spaces

TABLE OF CONTENTS

DEVELOPMENT SUBMISSION

1. Introduction - Development Team
2. Development Concept
3. Development Plan
4. Boston Residents Jobs Policy
5. Diversity and Inclusion Plan
6. Development without Displacement Plan
7. Additional Data

DESIGN SUBMISSION

DESIGN NARRATIVE

1. Proposal Design meets Development Objectives & Design Guidelines
2. Parking & Transportation and Circulation Plan meet Urban Design Guidelines
3. Preliminary Zoning Analysis
4. Proposal meets Resilient Development and Green Building Guidelines

DESIGN DRAWINGS

1. Neighborhood Plan 1"=40' scale and Site Plan 1'=20' scale
2. Schematic floor plans 1/8" = 1' scale
3. Building Elevations 1/8" = 1' scale
4. Street elevations 1/8"=1' scale
5. Perspective drawings drawn at eye-level and aerial views

FINANCIAL SUBMISSION

1. Formation Documents
2. Financial Documents
3. Financial Submission Workbook Template
4. One Stop - N/A
5. Financial Narrative
6. Ground Lease Price Proposal
7. Preliminary market study
8. Financing

DISCLOSURES

1. Transaction with a Public Agency
2. BPDA & City of Boston Disclosure
3. Tax, Employment Security & Contract Compliance
4. HUD Form 6004

SUBMISSION CHECKLIST

TEAM PROFILES

Development Submission



January 6, 2021

Ms. Teresa Polhemus
Executive Director/Secretary,
Boston Planning & Development Agency

Letter of Interest re: Pier 5 / Charlestown Navy Yard RFP

Dear Executive Director/Secretary Polhemus:

In response to the BPDA's Request for Proposals, please accept this letter as our formal expression of interest to be selected as the designated developer for Pier 5 in the Charlestown Navy Yard.

Our development entity, NAVY BLUE, is a tri-venture of Charlestown Marina, Parent+Diamond, and Urban Spaces (Partnership Agreement attached in Financial Section 1g.). Each partner has an equal 1/3 interest and brings to the partnership a unique set of skills and capabilities:

- Charlestown Marina has a seven-year history of working in the Navy Yard to develop and operate best-in-class marinas.
- Parent+Diamond, as an MBE / Application Pending, focuses on instilling diversity and inclusion, and will implement a platform with specific and proven strategies to provide opportunities for jobs and wealth creation for minority, women, and veteran-owned business and individuals.
- Urban Spaces has extensive experience with new construction, adaptive re-use, and historic preservation, both in the private and public sectors, and is an innovator that has advanced many urban trends, including micro-units and environmentally-sensitive design.

The NAVY BLUE team is committed to inclusion at every stage of the development cycle, including ownership, pre-development, construction, and ongoing marina operations. Approximately 65% of the firms on our team include ownership by minorities, women, and/or veterans. A full listing can be found in the attached team organizational chart.

We're delighted to participate and believe that our redevelopment concept will be the most effective, dynamic, and successful long-term approach for Pier 5 and the broader Charlestown community.

Best regards,

Navy Blue, LLC

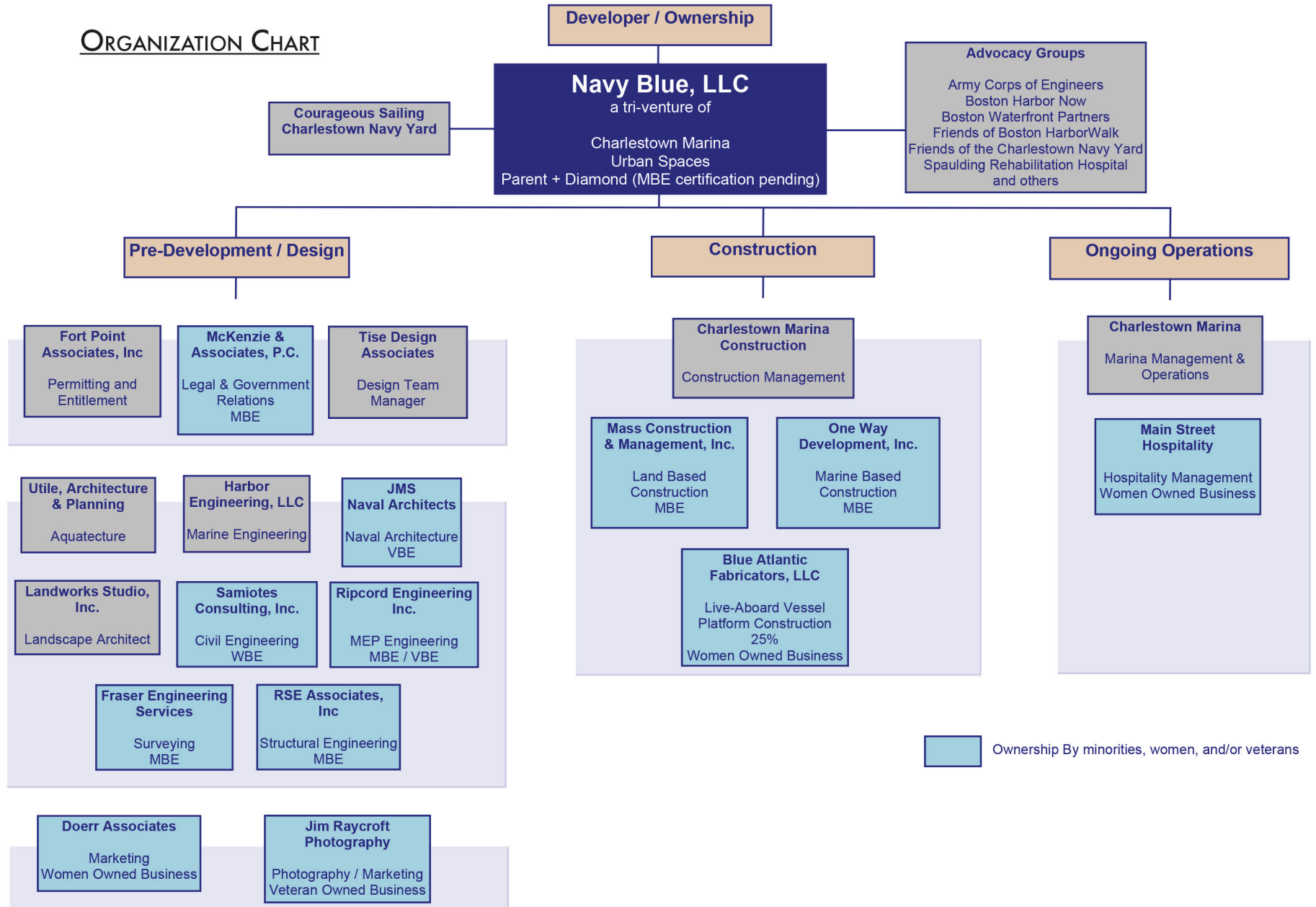
Charles Lagasse, Jr.
Charlestown Marina

Ralph Parent
Parent+Diamond

Paul Ognibene
Urban Spaces

1. INTRODUCTION

ORGANIZATION CHART



1. INTRODUCTION - DEVELOPMENT TEAM

a. Copy of partnership agreement (See Financial Section 1g)

b. Contacts for NAVY BLUE, LLC Partners

Chuck Lagasse	Ralph Parent	Paul Ognibene
Charlestown Marina	Parent+Diamond*	Urban Spaces
1 Pier 8, 13th Street	15 Greenock St	55 Bent Street
Boston, MA 02129	Dorchester, MA 02124	Cambridge, MA 02141
617-242-2020	617-304-5727	617-868-5558
clagasse@oceanhavens.com	ralph.parent@gmail.com	paul@urbanspacesllc.com
1/3 interest	1/3 interest	1/3 interest

*MBE – Application Pending

c. Explanation of relationship between any 3rd party developers, subcontractors, or community partners that might influence the development plan

NAVY BLUE is committed to fostering diversity and inclusion in every aspect of the development. Perhaps no clearer indication of this commitment is the Organizational Chart, included in this section 1a above, which provides a graphic representation of the relationship between the proponent, NAVY BLUE, and the project's consultants, subcontractors, and related community / advocacy groups, including Courageous Sailing and Friends of Boston Harbor Walk. In fact, all of the organizations listed on the chart had an influence on our approach to our RFP submission.

d. The proponent shall provide a listing/description of any lawsuits brought against the proponent or any principals of the proponent in courts situated within the United States within the past five years.

Neither the proponent, Navy Blue, LLC, nor any principals of NAVY BLUE have had any lawsuits brought against them in courts situated within the United States within the past five years.

2. DEVELOPMENT CONCEPT

- a. *Describe the proposed development's uses and the total square footage of each use, along with a description of how the proposed uses and design will satisfy the Development Objectives of this RFP.*

Our proposed redevelopment concept for Pier 5 emerges from a careful consideration of alternatives coupled with a confluence of timely factors relating to resiliency, technology, transportation (water taxi) and current real estate market in Boston.

NAVY BLUE development team is uniquely positioned to perform Pier 5 demolition without intruding on or disturbing direct abutters and the larger CNY Community. With 1,700 concrete pilings and two acres of deteriorated slab, demolition is a massive undertaking and will integrate recycling of the huge quantities of salvage material to protect local landfills. The current pier directly abuts Pier 6 and HarborWalk. Our plan is to remove Pier 5 from the waterside utilizing both Pier 5 and Pier 6 watersheets during off season, and transfer material to the East Boston Shipyard for grinding and recycling under controlled conditions.

We have assembled designers, engineers, marketers, and managers that combine extensive maritime and landside experience into a balanced team with the appropriate skillsets to make this concept a reality. In addition, our goal is to make this floating community as sustainable and "off the grid" as possible using the very latest passive and renewable strategies. Unlike seasonal marinas, our marina slips will have the integrated infrastructure required to support year round, full time, sustainable use. The total proposed marina capacity as designed accomodates up to 115,000 sf of live-aboard vessel space. In addition, the development concept includes the adaptive re-use and historic preservation of the 1,300 sf PumpHouse, a total of 116,300. To facilitate marina operations, such as bike storage, mail and package delivery, and other back of house functions we ventured into discussions with Flagship Wharf to secure access of 2,850 sf of commercial space.

2. DEVELOPMENT CONCEPT

b. Describe how the proposed development will benefit the surrounding community.

The Charlestown Navy Yard waterfront is an asset cherished by the residents of the Navy Yard as well as the larger Charlestown community. Many years of planning and funding by a wide variety of residents and stakeholders has resulted in an elegant, but incomplete canvas. The HarborWalk is the main artery of this asset, and our plan re-envision a significant section with innovative re-use and new vitality. At the same time, existing institutions are enhanced and expanded. We are uniquely positioned provide Courageous Sailing opportunities to enhance their operation. The water taxi will be strengthened by additional ridership, the community will enjoy meetings at the historic PumpHouse, and the retail base of Flagship Wharf will finally realize its potential.

Landward, the HarborWalk will extend Charlestown Marina's ongoing renovation of the HarborWalk between Pier 8 to Pier 6, by revitalizing the stretch of Harborwalk from Pier 6 to the historic Building 123 Pump House. By replacing the boardwalk, adding planters, seating, vegetation, swings, pergolas, and special interpretive features that can be programmed to tell a story of the historic Navy Yard and the Charlestown section of Boston. This new public way will become a model for the waterfront public realm. Improvements are primarily located along the sea wall, separating the on-grade paving from the wooden boardwalk. This special band references the edge of land and sea, the terra-firma edge of the Navy Yard, and will expand through the boardwalk as a timeline to mark moments of historical importance. Collateral enhancement will activate the terrace immediately adjacent to Flagship Wharf allowing retail to thrive, servicing the Charlestown Navy Yard and the linear park.

2. DEVELOPMENT CONCEPT

c. Estimate the number of construction and permanent jobs that will be generated by the proposed development.

- 1.) Renovations to the Harbor Walk between Pier 4 and Pier 6 –
Estimated Construction Time - 3-4 months

a.	Estimated construction jobs	
i.	Earth and site	8 FTE for 8 weeks
ii.	Carpentry	6 FTE for 8 weeks
iii.	Electrical	3 FTE for 4 weeks
iv.	Labor	3 FTE for 8 weeks

- 2.) Pumphouse renovations and expansion –
Estimated Construction Time – 4 months

a.	Estimated Construction Jobs	
i.	Selective Demolition	3 FTE for 2 weeks
ii.	Carpentry	4 FTE for 6 weeks
iii.	Masonry	4 FTE for 6 weeks
iv.	Roofing	2 FTE for 1 week
v.	Doors & Windows	2 FTE for 2 weeks
vi.	Electrical	2 FTE for 2 weeks
vii.	HVAC	2 FTE for 2 weeks
viii.	Plumbing	2 FTE for 3 weeks
ix.	Tile and flooring	2 FTE for 2 weeks
x.	Drywall	4 FTE for 3 weeks
xi.	Paint	4 FTE for 1 week
xii.	Labor	2 FTE for 6 weeks

- 3.) Pier Demolition -
Estimated Construction Time – 5 months

a.	Estimated Construction Jobs	
i.	Demolition	15 FTE for 5 months

- 4.) Marina Development and Construction -
Estimated Construction Time – 12 months

a.	Estimated Construction Jobs	
i.	Marine pile installation	10 FTE for 12 weeks
ii.	Carpentry	12 FTE for 25 weeks
iii.	Misc. Metals	8 FTE for 8 weeks
iv.	Electrical	6 FTE for 16 weeks
v.	Plumbing	6 FTE for 16 weeks
vi.	Labor	4 FTE for 20 weeks

- 5.) Live Aboard Vessels and Amenity Building Construction –
Estimated Construction Time 18 Months

a.	Estimated Construction Jobs	
i.	Barge construction	8 FTE for 25 weeks
ii.	Carpentry Rough	20 FTE for 30 weeks
iii.	Carpentry Finish	10 FTE for 16 weeks
iv.	Misc. Metals	6 FTE for 8 weeks
v.	Roofing	10 FTE for 12 week
vi.	Insulation	8 FTE for 8 weeks
vii.	Siding	8 FTE for 10 weeks
viii.	Doors & Windows	6 FTE for 10 weeks
ix.	Electrical	10 FTE for 30 weeks
x.	HVAC	8 FTE for 20 weeks
xi.	Plumbing	10 FTE for 25 weeks
xii.	Tile and flooring	8 FTE for 10 weeks
xiii.	Drywall	16 FTE for 20 weeks
xiv.	Paint	10 FTE for 16 week
xv.	Labor	8 FTE for 25 weeks

- 6.) Permanent Jobs Created

i.	Dock Management	4 FTE
ii.	Facilities Maintenance	8-10 FTE

2. DEVELOPMENT CONCEPT

- d. Proposals must include a narrative of the community benefits supported by the development, including any benefits to the local community that are above those generated by the development itself.*

Activation of vacant space in the ground floor of Flagship Wharf

Our plan is to create an administrative/reception office where residents and guests will transfer from parking in our reserved garage spaces to “green” carts that will take them to the marina. Because this administrative facility will be locally staffed, we are also proposing to integrate a model ship museum open to the public that complements the maritime focus of the Navy Yard. This will provide an interesting pedestrian opportunity as part of the HarborWalk experience.

Our proposal redevelops approximately 350 linear feet of HarborWalk running from Parcel 2C west to the intersection of Pier4

Our team has engaged LandWorks to reenvision this area with new paving, lighting, street furniture, and plantings that will be a major amenity for the entire Charlestown community. Our current vision assumes street furnishings will be fabricated along with our floating platforms at the East Boston Shipyard using local materials and labor.

The public realm associated with the floating community becomes an important asset to Navy Yard residents and visitors to the HarborWalk, extending the greenspace from the land and to structures floating on the water sheet. Aesthetically and ecologically, the planting of trees and native grasses will function as part of a filtering system along with natural air cleansing properties. The tree canopy will provide interest and compliment state-of-the-art architecture, completing the overall community aspect of the floating village. Pocket parks and street furniture will further enhance the vision for this model village sure to become a standard for future waterfront communities.

NAVY BLUE will redevelop the Pumphouse Building 123 into a meeting room for use by Courageous Sailing, groups including Friends of Charlestown Navy Yard as well as the larger Charlestown community

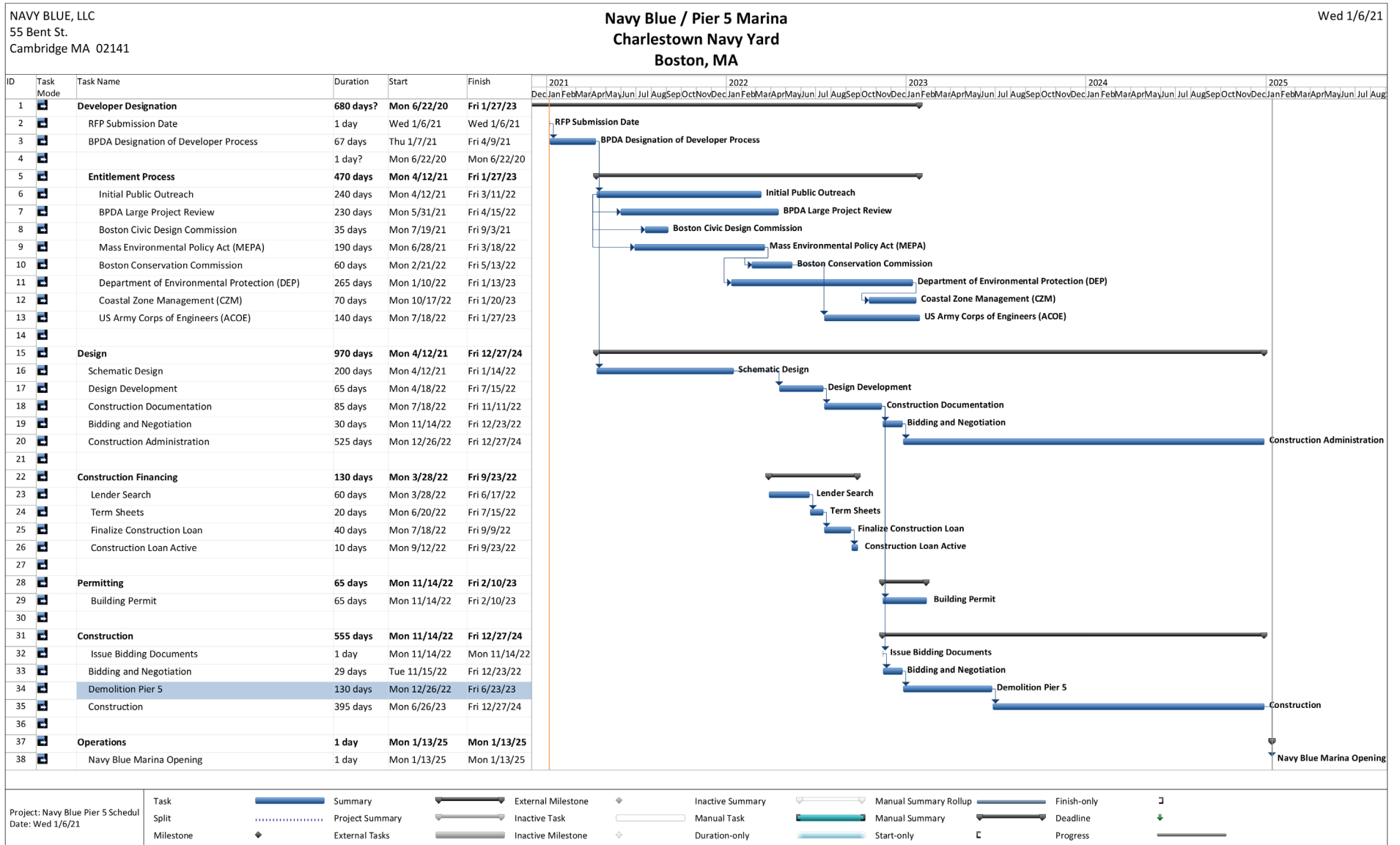
In consultation with Courageous, we are proposing to expand the footprint of the Pumphouse by appx. 500 sf to provide toilet facilities and food service opportunities in support of large gatherings. Located directly adjacent to the water taxi, this facility is easily accessible to the larger Boston populace.

We’ve agreed to work collaboratively to explore an array of ideas that would help facilitate CS’s mission, including:

1. Relocating CS’s non-powered vessels to the west side of Pier 4.
2. Sliding Pier 6’s property line to the east to enable an expansion of CS’s water sheet.
3. Renovating the Pump House into a community gathering space for use by CS, as well as local residents.
4. Bringing water, sewer, and other renovations to CS’s Pier 4 Chandlery building.
5. Jointly sponsoring educational opportunities regarding sailing, real estate development, and floating communities.

3. DEVELOPMENT PLAN

- a. Describe how the development concept will be implemented. The description should include a detailed timeline that lists all pre-development tasks from the date of Tentative Designation by the BPDA through loan closing and construction commencement. It should also indicate the start and end dates for each pre-development task.



3. DEVELOPMENT PLAN

- c. *Provide an outline of all required regulatory approvals and a projected timeline to obtain these approvals. The proponent should note the currently applicable zoning districts, overlays and provisions that govern development of the Property and discuss the type of zoning amendments or variances that are required for the proposed development, or indicate if the proposed development can be constructed “as-of-right” under existing zoning.*

Table 1 contains a list of federal, state, and local agencies from which permits or other actions may be sought or to which filings may be made.

Timeframe for completing pre-construction permitting and potential start of construction is approximately 18 months from Project initiation. This assumes that no variances are required from zoning, and that a Single Environmental Impact Report will be issued.

Table 1: Anticipated Permits, Reviews, and Approvals

Agency	Permit, Review, or Approval
Federal	
US Army Corps of Engineers (USACE)	<ul style="list-style-type: none"> Work in Navigable Waters (Section 10) Individual Permit
National Environmental Policy Act (NEPA)	<ul style="list-style-type: none"> Environmental Assessment/Finding of No Significant Impact
Federal Aviation Agency (FAA)	<ul style="list-style-type: none"> Determination of No Hazard to Air Navigation (cranes)
State	
Executive Office of Energy and Environmental Affairs (EOEEA)	<ul style="list-style-type: none"> Massachusetts Environmental Policy Act (MEPA) Review Expanded Environmental Notification Form(EENF) and Single Environmental Impact Report (SEIR)
Massachusetts Department of Environmental Protection (MassDEP)	<ul style="list-style-type: none"> Chapter 91 Waterways License Notification of Construction and Demolition
Massachusetts Office of Coastal Zone Management (CZM)	<ul style="list-style-type: none"> Federal Consistency Review
Massachusetts Historical Commission (MHC)	<ul style="list-style-type: none"> Finding of No Adverse Effect relative to historic and archeological resources Section 106 Review
Board of Underwater Archaeological Resources (BUAR)	<ul style="list-style-type: none"> Review of proposed work/Project Site to determine if Reconnaissance
	Excavation or Special Use Permit(s) are necessary

3. DEVELOPMENT PLAN

c. *(cont'd)*

Local	
Boston Planning and Development Agency	<ul style="list-style-type: none">• Article 80 Large Project Review• Cooperation Agreement• Certificate of Compliance• Lease Agreement• IGBC Article 37 Review• Compliance with Design Standards for New Development Area
Boston Conservation Commission	<ul style="list-style-type: none">• Order of Conditions under State Wetlands Protection Act and local ordinance
Boston Civic Design Commission	<ul style="list-style-type: none">• Review and Recommendation on Design
Boston Water and Sewer Commission	<ul style="list-style-type: none">• Site Plan Approval
Department of Neighborhood Development	<ul style="list-style-type: none">• Affirmative Marketing Plan
Boston Transportation Department	<ul style="list-style-type: none">• Transportation Access Plan Agreement• Construction Management Plan

3. DEVELOPMENT PLAN

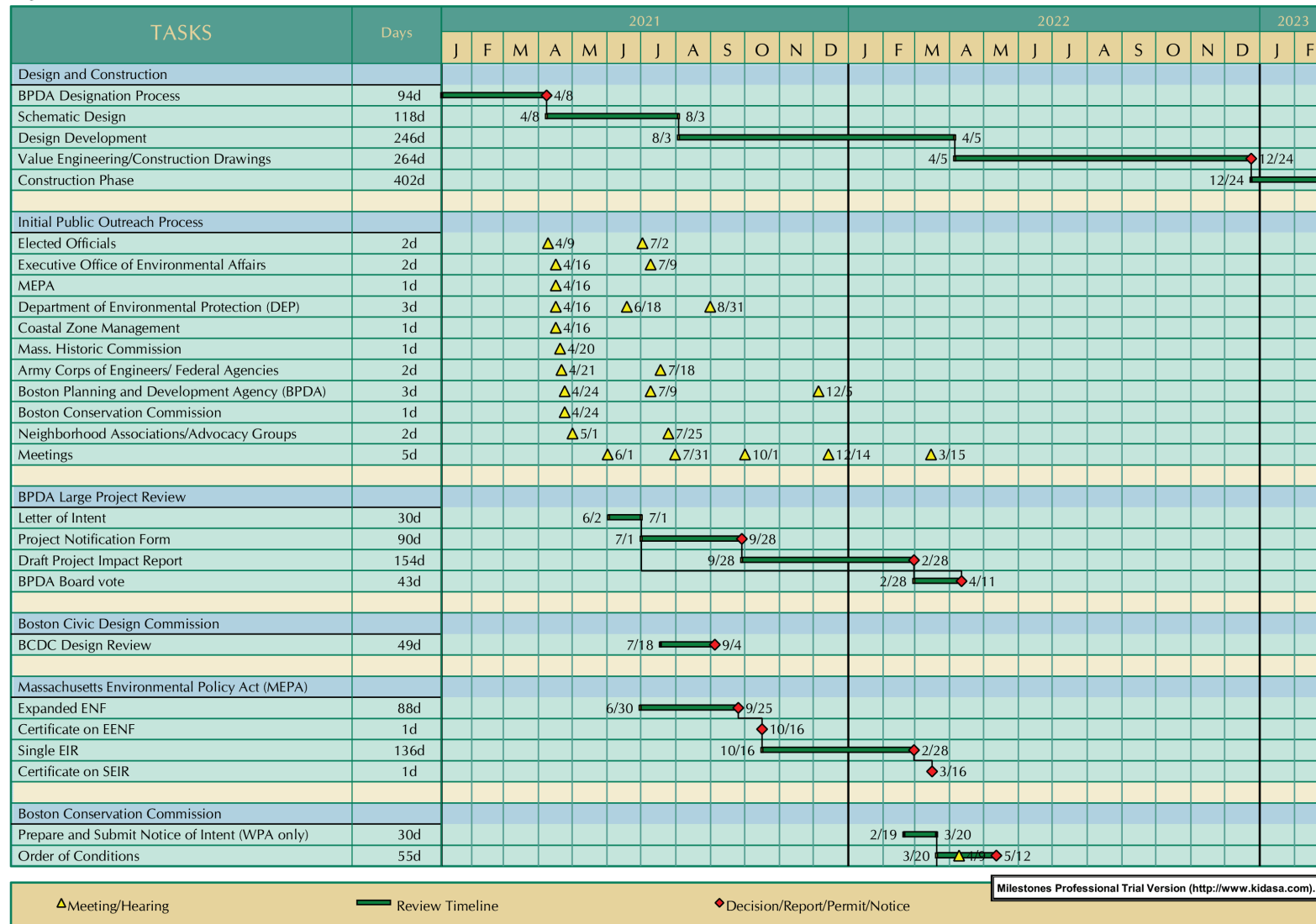


Fort Point Associates, Inc.
31 State Street, 3rd Floor
Boston, MA 02109
(617) 357-7044

Pier 5 Development Schedule

Page 1 of 2

12/31/20



3. DEVELOPMENT PLAN

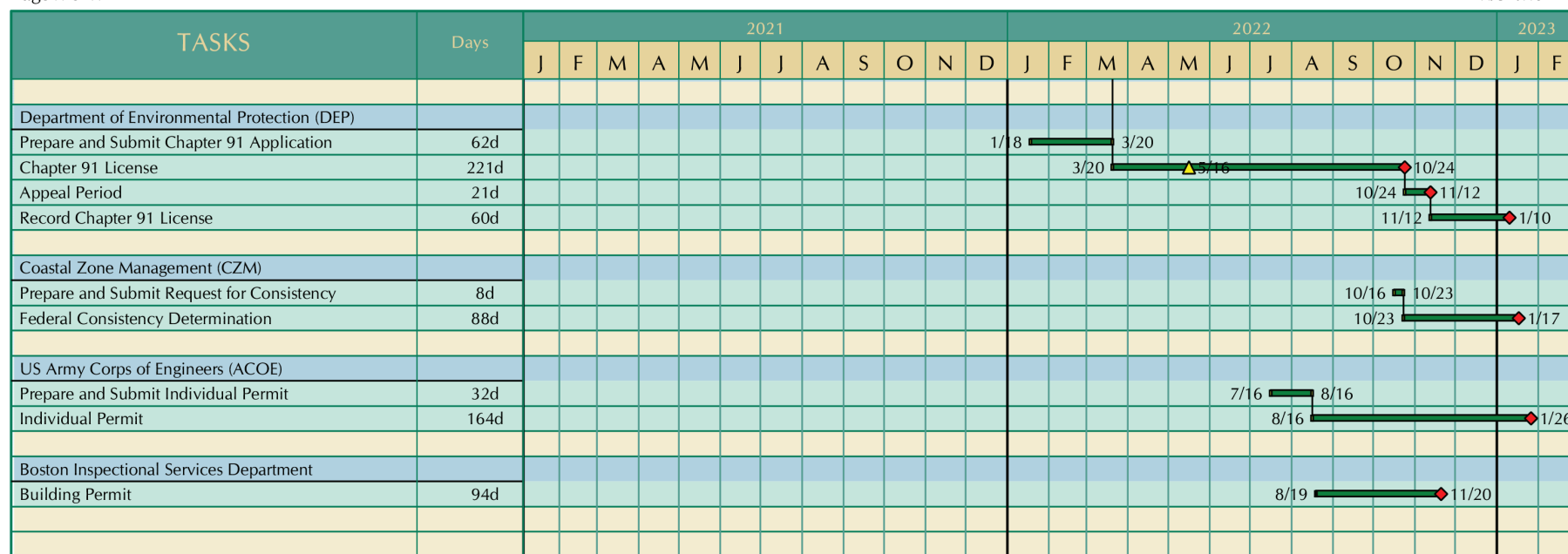


Fort Point Associates, Inc.
31 State Street, 3rd Floor
Boston, MA 02109
(617) 357-7044

Pier 5 Development Schedule

Page 2 of 2

12/31/20



▲ Meeting/Hearing

— Review Timeline

◆ Decision/Report/Permit/Notice

Milestones Professional Trial Version (<http://www.kidasa.com>)

4. BOSTON RESIDENTS JOBS POLICY

Boston Residents Jobs Policy. Proposals must describe the planned approach to meeting the goals outlined in the Boston Residents Jobs Policy (Appendix A)

The NAVY BLUE development team is very familiar with the City of Boston's Residents Job Policy consisting of:

- At least 50 % of the total work hours in each trade must go to Boston residents
- At least 25 % of the total employee work hours in each trade must go to people of color, and
- At least 10 % of the total employee work hours in each trade must go to women

It is our intention to surpass the minimum requirements of the Boston's Residents Job Policy and the construct of our team provides a direct connection to tradesmen and tradeswomen. For example, two members of our development team are on the Board of Directors of the Massachusetts Minority Contractors Association (MMCA). Maxime Charles, owner of Mass Construction & Management, Inc, serves as Vice-President of the MMCA and Abraham Gonzales, owner of OneWay Development serves as Clerk of the MMCA. We will rely on their leadership and membership to source qualified tradesmen and tradeswomen. Blue Atlantic Fabricators is a member of our team and has a strong history of hiring local minority employees.

These are not merely goals and objectives to our team, but a way of working that has typified the three firms that comprise the partnership of NAVY BLUE. Section 5, Diversity and Inclusion, encompasses some of the unique strategies we've used over the years and to this day in order to to exceed minimum quotas in every category.

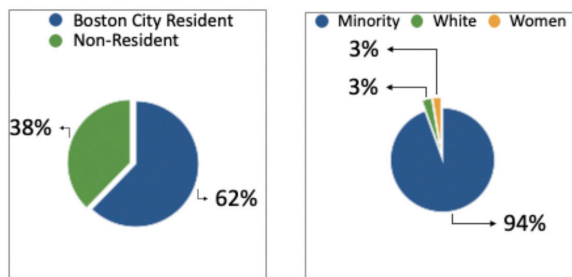
The following charts demonstrate the resident compliance for two of the firms on our team with the Boston Resident's Job Policy.

4. BOSTON RESIDENTS JOBS POLICY



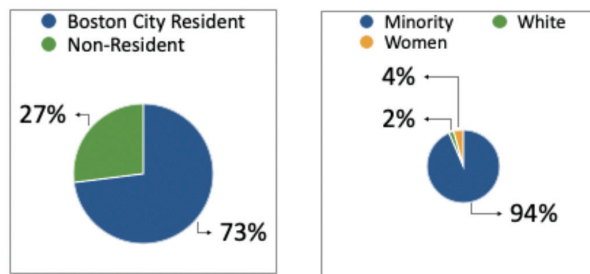
Project Name:
Department of Neighborhood Development
Glenway Roxton

Description:
Boston Residency Jobs Policy Statistics



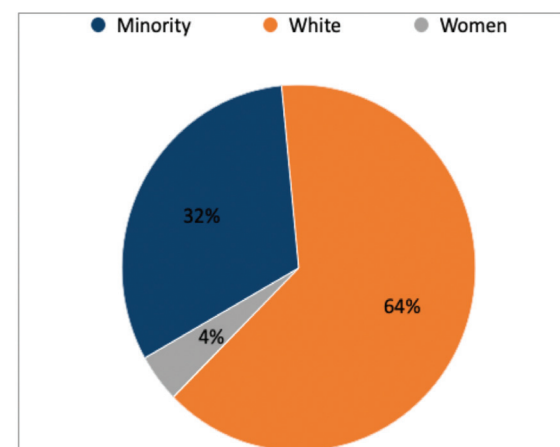
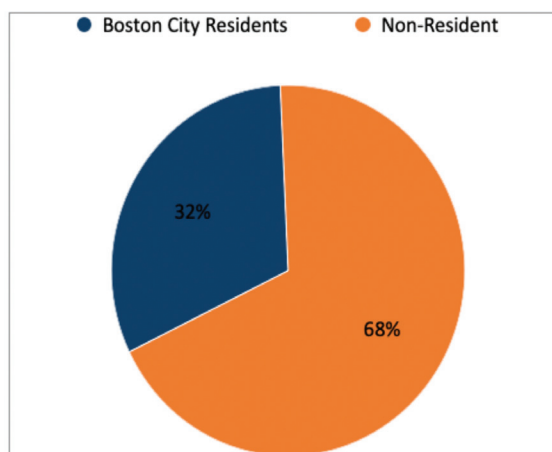
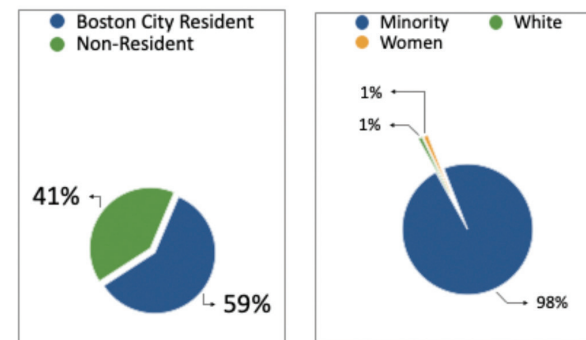
Project Name:
Department of Neighborhood Development
Edison Wentworth

Description:
Boston Residency Jobs Policy Statistics



Department of Neighborhood Development
Mattapan Hiawatha

Description:
Boston Residency Jobs Policy Statistics



5. DIVERSITY & INCLUSION PLAN

A. Introduction – Our Commitment

Our Diversity & Inclusion (“D&I”) Plan is as unique as our floating community marina concept. It’s also very timely. Building on decades of doing voluntarily what is now essential, our team is eager to ensure that NAVY BLUE advances equity and inclusion in Boston. To that end, our team has designed a specific plan to create job opportunities and wealth opportunities for minority businesses and individuals who have historically been underrepresented in real estate development.

The NAVY BLUE team is committed to exceeding expectations regarding the inclusion of people of color, women, and MBE, WBE, and VBEs in every aspect of the development process and beyond. We do not simply “intend to...” nor will we “use best efforts to...” accomplish the objective. Instead, we are committed to creating, refining, implementing, and reporting our activities in accordance with the Diversity & Inclusion Plan described herein.

In fact, the NAVY BLUE team will mirror BPDA / Boston’s pioneering efforts in this regard as it works to create a template for other municipalities to advance equity and inclusion.

B. The Past – NAVY BLUE History with Diversity & Inclusion

Decades ago, two of the members of the NAVY BLUE development team, Merrill Diamond and Stephen Tise, were immersed in HUD’s HOPE VI Program, a massive holistic approach to the revitalization of distressed public housing developments across the country. The program combined physical enhancements with social and economic initiatives in order to improve the lives of our country’s poorest citizens.

The first of the HOPE VI projects to be completed were Concord Village and Eagle Creek, two public housing developments in Indianapolis, Indiana, completed by Diamond and Tise. The project was designed and developed by working with minority development and design partners and, for the first time in the program’s history, 35 disadvantaged minority general contractors and their minority subcontractors performed all of the construction of hundreds of affordable, single-family homes. Our plan ensured that HOPE VI money re-circulating circulated in the Near Westside community instead of it going to majority, out-of-town contractors and vendors. This approach to the HOPE VI implementation became known as “The Indianapolis Approach” and we intend NAVY BLUE to implement similarly.

(An illustration of the Concord Village project and a Letter referencing the activities of Diamond and Tise in Indianapolis can be found at the end of this section.)

5. DIVERSITY & INCLUSION PLAN

C. The Present – NAVY BLUE Involvement with Diversity & Inclusion

More recently, minority business owner, Ralph Parent, teamed with Diamond to found Parent+Diamond (“P+D”), a Dorchester-based MBE (application pending), to bring to Boston the Indianapolis Approach noted above. Parent is a resident of Dorchester and was an All-American football athlete at Brookline High School, stalwart defensive star for the Boston College Eagles, and a professional football player, both in Europe and in the NFL. Parent has extensive experience in brokerage, navigating through the city’s permitting process, and responding to RFPs.

Working in joint venture with Urban Spaces, another member of the NAVY BLUE tri-venture, P+D was recently selected by the city’s Department of Neighborhood Development to develop 40 affordable homes, singles and two-families, on vacant lots scattered throughout Roxbury, Dorchester, and Mattapan. Our JV, called the Boston Partnership for Community Reinvestment (“BPCR”), is focused on developing using MBE, WBE, VBE, and other local, firms. We are ensuring that the development process creates jobs and wealth opportunities within our host neighborhoods.

In doing so, we made the decision to not simply “give jobs,” but to cultivate and nurture opportunities to assist minority business owners to develop their corporate platforms. For example, Kurt Fraser of Fraser Engineering (MBE) and Maxime Charles of Mass Construction & Management, Inc (MBE) are on the development team for BPCR. With the goal of growing our companies together-- and in fact our diversity and inclusion (D&I) platform-- Fraser and Mass Construction are also part of the NAVY BLUE team.

If the past is truly prologue, the City can expect us to not only bring our tested and proven D&I strategies to Charlestown, but to enhance them in order to impact beyond the boundary limits of Pier 5 itself.

Charlestown Marina, a member of the NAVY BLUE tri-venture, also has a long history that is immersed in diversity and inclusion. While operating for nearly seven years in the Charlestown Navy Yard, Charlestown Marina has embodied a voluntary and continual minority outreach effort, both identifying and hiring Boston residents and minority businesses throughout Charlestown and Boston. Ann and Chuck Lagasse, owners of Charlestown Marina, have reached out to the community in other ways as well, including a partnership with Impossible Dream, which provides free dockage and assistance to the patients of Spaulding Rehabilitation Center, as well as access to a completely accessible vessel. In addition, Ann Lagasse coordinates Charlestown Marina’s outreach programs, and serves as a trustee of Boston Harbor Now and is board chair of the Joslin Diabetes Center.

5. DIVERSITY & INCLUSION PLAN

D. The Future – The NAVY BLUE Diversity & Inclusion Platform

Mission Statement

The mission of the NAVY BLUE development team is to develop the most dynamic and successful long-term approach for the redevelopment of Pier 5, with a commitment to creating jobs and wealth opportunities for businesses and individuals who have historically been underrepresented in real estate development. (make bold?)

Working with all parties with a vested interest in this D&I program, we will create a publicly-shared template that embraces strategies that work best for accomplishing the program's objectives. As with other aspects of our proposal, NAVY BLUE sees itself as a true partner of the BPDA and the Charlestown community and we will be transparent and open to both criticism and helpful suggestions for improvement.

Community Outreach

The NAVY BLUE team will initiate a broad community outreach program aimed at people of color, women, other minorities, and veterans. This outreach will promote the NAVY BLUE Apprenticeship Program whereby interested local residents, including recent graduates of Charlestown High School, would be provided with the opportunity to learn by working on fabricating the components of the project under experienced supervision focused as much on teaching as on building.

The outreach will also promote the NAVY BLUE Jobs Sourcing Initiative, which seeks to create job opportunities within some of Boston's most underserved communities. This has been one of the major challenges to the type of work that we and others have been doing in the area of D&I. Our solution to that problem has been and continues to be in the hands of Parent / P+D. Parent's efforts in identifying minority individuals and companies have been essential to our efforts. It is Parent's talent in this regard that will ensure the success of NAVY BLUE's D&I Plan.

Another outreach strategy will be NAVY BLUE's "BUILDING A FLOATING COMMUNITY" program, which will be a variation of our successful "BUILDING A BUILDING" program that we created a number of years ago and first implemented for the pre-engineering classes at Brighton High School. The BUILDING A BUILDING program was a curriculum that followed the course of construction at our nearby Lancaster development, a Department of Neighborhood and Development ("DND") sponsored project located at 1501 Commonwealth Avenue in Brighton. Utilizing both classroom and on-site presentations, speakers presented students with a snapshot of every phase of development, from pre-development through sales and marketing, to provide a palette of career possibilities to interested juniors and seniors. We invite the reader to contact Daniel Cuddy of Brighton Marine for an evaluation of the program.

At the CNY, we hope to adopt this program to unravel the technicalities of marine construction and the expanding challenges of resilient, waterside development. We have discussed this possibility with Courageous as potential co-sponsors, and also considered expansion of the program to include actual internships and mentorships with NAVY BLUE team members. Access to the East Boston Shipyard by participants in "Building a Floating Community" will create a hands on learning experience in marine trades for Boston youth.

5. DIVERSITY & INCLUSION PLAN

D. The Future – The NAVY BLUE Diversity & Inclusion Platform (cont'd)

3. Proven Strategies for Success

One of the most powerful strategies we have used and will utilize with the development is NAVY BLUE Launch which is our approach to partnering smaller local MBE, WBE, and VBE companies with larger more established firms. We are committed to working through bonding, insurance, and other issues in order to ensure participation of firms that may not otherwise have access and to assist their growth in size, reputation, and capabilities. (See org chart in Section 1.)

We are committed not only to creating employment opportunities during the construction phase, but also to maximizing the number of permanent jobs that will stem from the development's ongoing marina operations.

4. New Strategies for Success

The NAVY BLUE team will draw on our own strategies that have proven successful, but will also engage in new approaches. For example, we will provide a mechanism for constituents to make micro-investments, which will enable broader participation in the project's ultimate success. We will also institute a Diversity and Inclusion Reporting Protocol that will serve as a dashboard to help monitor or progress.

E. Conclusion

The NAVY BLUE development team fully embraces the City of Boston's initiatives to foster diversity and inclusion. We're excited to play a small role in the BPDA's approach, which we believe will become a template that will be adopted by municipalities across the country.

Perhaps no clearer indication of the commitment made by the NAVY BLUE team to fostering diversity and inclusion appears in our Organization Chart included in Section 1. It provides a snapshot of the integration of populations whom Section 5 is intended to help along with the relationships to their joint venture partners within every aspect of the overall development.



5. DIVERSITY & INCLUSION PLAN

CONCORD VILLAGE / EAGLE CREEK



Project Description:

Concord Village and Eagle Creek was the result of the comprehensive revitalization of two distressed public housing developments, including complete demolition of the existing housing stock, new construction of 170 on-site dwelling units, and 50 scattered site infill houses and new public streets and sidewalks. Supplementing this is a new 15,000 S.F. Community Center including a 70 child daycare center, new open space and recreational opportunities and extensive social services and management improvements. Unit sizes are the following: 1BR - 660 S.F., 2BR - 1166 S.F., - 3BR 1350 S.F., 4br - 1450 S.F., 5BR - 1680 S.F. In addition, 5% of the units are fully accessible.

Design Process:

Extensive interactive design workshops were carried out with residents to determine the overall planning strategy, unit plans, and building designs utilizing "kit of parts" models and multiple options during both the masterplan and schematic design phase of the project.

Design Solution:

Renovation and townhouses strategies were explored before a "traditional neighborhood strategy" was adopted by residents to reflect the scale and character of the of the surrounding historic 1920's era neighborhood. The design integrates the formerly isolated development into neighborhood by extending the grid of public streets through the site to form a traditional block pattern. Sixteen building types were developed to maximise identity and sense of place for the residents. The mix of both duplex and single family homes are designed to reflect the typology of the neighborhoods craft style bungalows. Corner duplex units are designed with full porches; as the resident strongly expressed the importance of the porch as both a welcoming space and as a "outdoor room" in warmer months.



INDIANAPOLIS HOUSING AGENCY INDIANAPOLIS, INDIANA

TH&D/WOODS - a joint venture
Program Management, Planning, Architecture

• U.S. DEPARTMENT OF HOUSING AND URBAN DEVELOPMENT
• INDIANAPOLIS HOUSING AGENCY
• ARMONICS INC. - CONSTRUCTION CONSULTANTS

Tise, Hurwitz & Diamond
200 Washington Street
Brookline, MA 02146

Clyde Woods & Associates
2000 North Capitol Avenue
Indianapolis, IN 46208

PROJECT TEAM:
Merrill Diamond Developer
Stephen E. Tise A.I.A.
Clyde Woods R.A.
Don Delmon R.A.
Russell Lowe R.A.
Carol Hill
Stephen E. Tise Jr.
Shu Tong Wong R.A.



Five Indiana Square
Indianapolis, IN 46204-2001
Phone: (317) 327-8100
Fax: (317) 327-8110

Edward R. Jagannathan
Executive Director

Martin D. Williams
Deputy Executive Director

COMMISSIONERS

W. Lew Fields, Chairman

Lois M. North, Vice-Chairman

Chester B. Carpenter

Ronald V. Franklin

Aaron E. Maith

Virginia B. Ryder

David C. Sease

October 28, 1996

Re: Tise, Hurwitz & Diamond, Inc.

To Whom It May Concern:

I am writing this Letter of Reference to speak to my experience with the firm of Tise, Hurwitz & Diamond, Inc., a Boston-based Program Management, Planning, and Development Consulting firm with which I am currently working in my capacity as Executive Director of the Indianapolis Public Housing Agency.

Although the firm has been active in Indianapolis since 1994, my experience with them dates from my appointment as Executive Director in January of 1996. At the time of my arrival, the firm had completed a HOPE VI Revitalization Plan for Concord Village and Eaglecreek, two Indianapolis public housing developments. This plan had been submitted to, and had received approval from HUD in the fall of 1995, along with a HUD waiver authorizing TH&D, in joint venture with an Indianapolis-based MBE firm, to implement the plan utilizing a Program Management approach. My direct contact with the firm was through the senior partner of the Program Management joint venture, Mr. Merrill H. Diamond of Tise, Hurwitz & Diamond, Inc.

In the nine months from my appointment as Executive Director, to the writing of this letter, the project has made enormous strides in bringing sustainable revitalization to the residents of Concord Village and Eaglecreek, and the surrounding community, the Near Westside of Indianapolis. In addition to Indianapolis' Hope VI program being further along than most, it is also one of the most unique HOPE VI projects in the country. Featuring a physical approach, which embodies all of the principles of the New Urbanism and a Low Income Housing Tax Credit program to further homeownership opportunities for public housing residents, the project is especially distinguished by its maximizing the participation of local, disadvantaged contractors and builders in the actual on and off-site construction - essentially creating an economic development initiative from the construction process

Mr. Diamond and his firm have been instrumental in the development and execution of all aspects of this program, including physical, social and management planning, economic development, and financial and development consulting. He has been personally committed to me and the program, and has worked tirelessly, sometimes in the face of seemingly insurmountable opposition and obstacles, to both create, implement, and explain the Indianapolis HOPE VI project. It is fair to say that much of our

486

CITY OF INDIANAPOLIS
STEPHEN GOLDSMITH, MAYOR

6. DEVELOPMENT WITHOUT DISPLACEMENT PLAN

- a. *Proponents must include a narrative explaining how their proposal supports the community’s goal of “development without displacement.” Specifically, this narrative should address how the proposed development will assist the current residents of Charlestown to remain in their community in the future, afford housing, and find pathways to economic opportunity. At a minimum this narrative should include the affordable housing production goals of the project and articulate how the proposed rents meet the needs of Charlestown residents, as well as other local residents. This discussion should also identify how proposed sizes of units meet the needs of community members, taking into consideration that community members have suggested that larger unit sizes of two, three and four bedrooms are needed for local families, while smaller unit sizes may be appropriate for seniors.*

The development parcel consists of an existing and deteriorating Pier 5, the adjacent water sheet, portions of the Harbor Walk between Pier 4 and Pier 6, and the existing Pump House just north of Pier 4. These selected areas are vacant and not used for residential purposes. As such there will not be any displacement.

- b. *The development team’s track record for supporting projects and policies which promote development without displacement should also be included.*

Although unrelated because this proposal will cause no displacement, the NAVY BLUE team has a long track record of promoting residential development without the displacement of local residents.

7. ADDITIONAL DATA

Any other relevant information the proponent believes is essential to the evaluation of the proposal (i.e., aesthetic designs, environmental sustainability goals, property management plans, ideas for selection of subcontractors, methods of obtaining community engagement, etc.)

N/A

Design Submission

DESIGN NARRATIVE

1. *A written and graphic description explaining how the proposed design will meet the Development Objectives and Design Guidelines of this RFP. These documents must describe and illustrate all program elements and the organization of these spaces within the building.*

The live-aboard vessels (LVs) proposed for NAVY BLUE will be fabricated and assembled at the East Boston Shipyard. The designs employ full steel monohull platforms, allowing for a deep draft and heavy displacement of the superstructure, resulting in enhanced stability. The platforms will be constructed by Blue Atlantic Fabricators LLC, a well regarded 25% women owned tenant in the Shipyard. They will be epoxy coated and fitted with anodes to mitigate corrosion. Anticipated hauling for cleaning and repair will also occur at the Shipyard on an anticipated 20 year cycle.

The LVs will be classified as USCG certified vessels, and the requisite oversight during hull fabrication will be in place. Each platform will be equipped with an outboard mount to allow for movement when servicing, marina maintenance, or as other needs dictate.

Superstructures will be highly energy efficient, with Passive House being the ultimate goal. Large expanses of triple glazing will be complemented with a variety of composite siding systems. Moisture resistant materials will be used throughout, and all metals will be stainless steel, coated aluminum, or galvanized steel. Interior finishes will be ultra contemporary and include a wide variety of premium materials. All mechanical systems and appliances will be electric, including induction cooking, electric fireplaces and air source heat pumps for heat and AC. Roof water may be stored in the platform hulls and recycled for grey water usage. Solar panels will be fully integrated into roof designs.

Currently we envision as many as 6 basic prototypes, with living quarters sizes ranging from 530 sf to 2100 sf individual units. Each prototype is two levels, with a third level head house accessing a roof terrace in a variety of configurations. Detail design will commence immediately after designation.

The suggested improvements to the stretch of HarborWalk from Pier 4 and Pier 6 will be designed in keeping with the overall HarborWalk design standards established by the City in collaboration with stakeholder groups. These will include interpretative panels and kiosks, as well as seating areas, landscaping, and viewing opportunities. Special provisions for people with disabilities will be provided, in recognition of Spaulding Rehabilitation Center's adjacency and other recently improved sections of the HarborWalk.

DESIGN NARRATIVE

The marina infrastructure will be constructed of materials similar to those used at Pier 6. Our current plan is to employ a 16' wide platform/attenuator at the end of our dock system to mitigate wave action. All floating dockage will be secured with tubular steel pilings and framed with select lpe decking on polyethylene floats. Dockage will be heated in the winter and will contain full utilities including water, forced main sewage, and electricity/internet/cable. LED light bollards will be Dark Sky compliant, and the "dockscape" will include benches, planters, bicycle racks, and other forms of traditional street furniture.

We have allocated a number of open air amenity areas for residents to gather and socialize. These will be equipped with fire pits, planters, and media sources to promote extended seasonal use.

As stated elsewhere, we are also proposing an amenity pavilion at the entry point where the marina meets the HarborWalk. This is currently indicated as a floating element but will require further study of the residual landside portion of Pier 5, which extends under the HarborWalk. This pavilion will serve as the marina community's amenity space as well the water taxi stop. The marina and improvements will meet or exceed all applicable State and Federal requirements for accessibility.

2. A description and illustration of the bicycle parking, automobile parking and transportation and circulation plan for the proposed development based on the Urban Design Guidelines set forth in this RFP.

NAVY BLUE will provide 112 indoor parking spaces located at the Flagship Wharf garage. Residents and guests will meet in a reception center in the base of Flagship Wharf and, if requested, be transported to the marina slip in an electric cart driven by a marina employee. They can also walk to their slip location, or use bicycles on the Harborwalk and marina docks, where bicycle racks will be integrated into the marina float system. Our amenity center will also have bicycle storage, e-bike charging stations, and bicycle rental opportunities.

The Charlestown Ferry stop is nearby and the Watertaxi service readily available. Opportunities for dockage will be available throughout the marina. MBTA regular bus service (92 and 93) to downtown is nearby and North Station is within walking station..

DESIGN NARRATIVE

3. *Preliminary Zoning Analysis*

Harborpark District - Charlestown Navy Yard (Pier 5)

42F-5 Sets forth the standards for the BPDA to issue a Section 18 finding for a Chapter 91 license. The project complies with these standards in the following manner :

1. The Project serves a proper public purpose by providing visual and physical access along the waterfront, solving the challenge of a decayed pier no longer open to public access, complementing the historic character of the Navy Yard through appropriate redevelopment and avoiding any adverse impact to navigation.
2. The Project will develop a Pedestrian Access Network to provide continuous lateral access along the shoreline and provide appropriate pedestrian amenities and interpretive signage.
3. The Project is in close proximity to both water taxi and water shuttle docking facilities and can accommodate water taxi vessels on site.
4. Only vessels will be allowed to dock in the marina.

note: Sections 3, 4, and 5 of 42F-5 are inapplicable as the Project is a marina

42F-6 - Open Space - 50% open space requirement on all land or piers in the Charlestown Navy Yard Subdistrict is not applicable to marinas or floating dock space.

42F-7 - Water Yard Area Requirements - Setbacks of 12' at sides of piers and 35' at ends of piers nor applicable to Marinas or floating docks.

42F-8 Urban Design Guidelines - Because our project is proposing a Marina to accommodate vessels, our project will comply with:

1. traditional pattern, height, and massing of the urban waterfront.
2. directing views and pedestrian movements towards the water
3. avoiding continuous walls parallel to the water's edge by maintaining view and access corridors
4. stepping down massing in height toward the water's edge, except along the Little Mystic Channel.
5. massing shall enhance the air flow channels created by sea breezes that are beneficial to air quality in the city.
6. terraces, gardens, arcades, and similar elements shall be designed to enhance pedestrian activity, access to, and enjoyment of the waterfront.
7. designed to minimize the visibility of roof structures and penthouses normally built above the roof and not designed to be used for human occupancy.
8. pedestrian pathways connecting to the waterfront and, where appropriate, linking the waterfront and mass transit stations;

DESIGN NARRATIVE

3. Preliminary Zoning Analysis (cont'd)

9. spaces accommodating pedestrian activities and public art;
10. use of materials, landscaping, public art, signage, lighting, and furniture that enhance the pedestrian and waterfront environment;
11. pedestrian systems that encourage more trips on foot to replace vehicular trips;
12. other attributes that improve the pedestrian environment and pedestrian access to the waterfront and Boston Harbor

42F-9 Environmental Protections and Safety Standards

1. The project will house trash and recycling for the marina in appropriately sized interior spaces with proper ventilation.
2. The project will provide safety ladders at the water's edge per this section of the code.

42F-10 - Off Street Parking

1. Off street parking will be accommodated in the adjacent Flagship Wharf property.
2. Parking counts will be determined through the Article 80 process.

42F-12 - Off Street Loading

1. Will be accommodated at the adjacent Pier 6 and will be serviced by shuttle vehicles able to navigate the marina dock structures.

42F-13 Dimensional Regulations

1. Maximum new gross square feet floor area
 - a. Allowed - 245,000 (provided not more than 550,000 previously existing)
 - b. Proposed - 113,000 new GSF in the form of water borne vessels.
2. Maximum height
 - a. Allowed - 75' general and 55' when less than 150' from end of pier.
 - b. Proposed - Less than 35'

42F-14 - Use - Pier 5

1. Allowed - Residential, Marina amenity Use
2. Proposed - Residential, Marina amenity Use

42F-17 - Rights to Navigation

1. All Rights to navigation will be maintained.

DESIGN NARRATIVE

4. A written and graphic description of how the proposed development will satisfy the Resilient Development and Green Building guidelines of this RFP that includes:

a. The team's approach to integrated project design and delivery

Navy Blue LLC has assembled a team unparalleled in the development of water and land-based development. Our team is extensive and inclusive of all disciplines needed to make this a world class project. Many of the team members have collaborated on other successful projects and are eager to collectively make this project a model for future floating villages.

The success of this project is rooted in the involvement and acceptance of the design by stakeholders beyond the development team. First in the process is clear and detailed programming of the project for acceptance by the BPDC and all other permitting agencies. Front-end meetings will assure that the design team is approaching the project to meet all goals established by regulating agencies.

Additionally, through public workshops, we will involve the immediate and wider Charlestown neighbors, including the residents of Charlestown, National Park Service and other Charlestown Navy Yard businesses. The collaboration with various groups will allow design to respond to the needs of stakeholders and assure buy-in of the final product. As opportunities become available for public presentation, the facilities of the Navy Yard may become instrumental in facilitating the public dissemination of information.

Team coordination and continuous review to City Agencies is paramount to the project success. Our team will be conducting weekly coordination meetings and larger Agency reviews will be scheduled throughout the design process. The design and construction schedule for the project is fairly short, so it is imperative that all participants remain actively updated and reviews are immediate.

The construction and assembly of the docks, LVs, linear park amenities and all appurtenances is equally important to the process of making NAVY BLUE a reality. Charlestown Marina will act as the General Contractor for the project, working from their East Boston shipyard facility. Components will be fabricated, assembled, and shipped directly to the site from East Boston, allowing for continuous oversight and inspection, while limiting time often accrued for logistics and shipping.

DESIGN NARRATIVE

b. Zero Carbon Building Assessment including performance targets for energy use and carbon emissions (or Home Energy Rating System (“HERS”) index score)

Utile conducted a preliminary energy model to evaluate the energy efficiency of a proposed residential development in Pier 5, Charlestown. The project is designed to be a model for energy efficient, resilient water-based living. This study assesses how a prototype is designed to be Zero Net Carbon Ready and how it can reach Zero Net Carbon. Modeling was conducted using BEopt v2.8.0, which uses the Energy Plus simulation engine.

Energy Summary

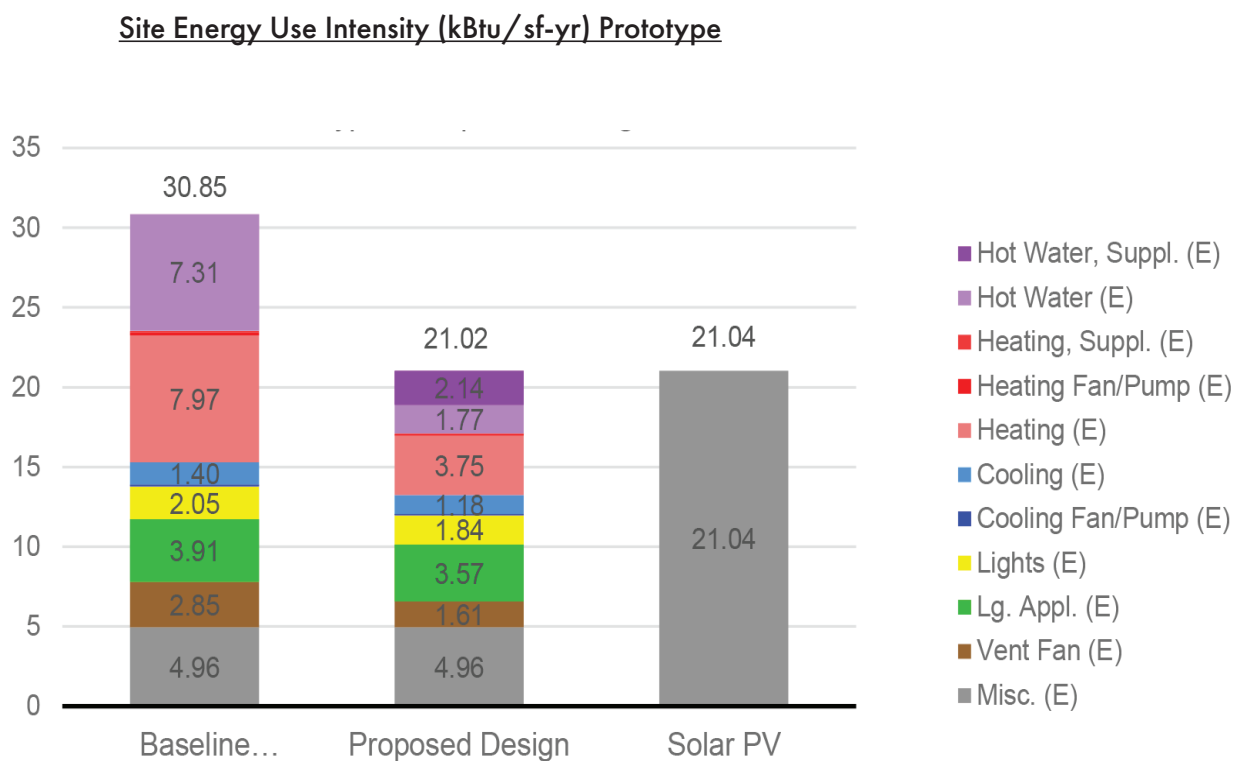
As shown in Figure 1, the proposed design represents a 32% improvement over the MA Stretch Code baseline without PV. Findings of this preliminary model are described below, with assumptions outlined in Table 1:

- Envelope improvements, including adding exterior continuous insulation, upgrading to high performance triple-glazed windows, and improving roof/floor insulation greatly reduce heating loads. Additional levels of air tightness beyond code can provide further savings but are not included in this comparison in keeping with modeling conventions.
- Water heating is a major energy end use, so integrating conservation measures will lead to a significant reduction in energy use. These include upgrading to a heat pump water heater, low-flow fixtures, and efficient pipe distribution runs.
- Providing an efficient ERV with a high sensible recovery ratio and low W/cfm fan power rate is important to reduce peak and annual energy loads.
- Low lighting power density (LPD) and efficient appliances can further drive down energy usage. For example, high efficiency heat pump dryers can reduce consumption and eliminate venting.
- All-electric systems position the proposed design to be zero carbon ready, because of the elimination of on-site combustion and the reduced loads because of the design’s efficiency measures.
- About 6 to 6.5 kW of on-site PV can cover 100% of each proposed dwelling unit’s energy usage, on average. This represents about 18 solar PV panels per unit, which can be integrated on the roof of each building either through direct rack-mounting or via integrated canopies.

DESIGN NARRATIVE

b. Zero Carbon Building Assessment (cont'd)

Figure 1: Site Energy Use Intensity (EUI) Analysis – Prototype



DESIGN NARRATIVE

b. Zero Carbon Building Assessment (cont'd)

The following table summarizes the energy model inputs for the Baseline and Proposed cases:

Table 1: Inputs Comparison of Proposed Design Versus Baseline Design

Building Component	Baseline: IECC 2018	Proposed Design (Zero Net Carbon Ready)	Zero Net Carbon		
Gross floor area	1150 sf	Same as Baseline	Same as Baseline		
Walls	R-13 + 7.5 c.i.	R-34 effective: R-6 mineral wool in service cavity; R-22 Mineral wool in stud cavity, R-12c.i. Mineral Wool (2")	Same as ZNC-Ready		
Ceilings/Roofs	R-38 c.i.	R-60	"	"	"
Floors	R-30	R-40	"	"	"
Window-Wall Ratio	15%	Front: 40%; Back: 50% Sides: 10%	"	"	"
Windows	U-0.30, SHGC 0.40	U-0.18, SHGC 0.35	"	"	"
Air Leakage	3 ACH50	3 ACH50	"	"	"
Mechanical Ventilation	Balanced ventilation, ERV sensible efficiency = 0.5 Electric eff. = 1 W/cfm 80cfm total	Balanced ventilation, ERV sensible efficiency = 0.85 Electric eff. = 0.5 W/cfm 80cfm total	"	"	"
Heating	Electric, Mini-Split Air Source Heat Pump HSPF = 8.2	Electric, Mini-Split Air Source Heat Pump HSPF = 12	"	"	"
Cooling	SEER = 14	SEER = 22	"	"	"
Cooling/Heating Set Point	70F / 77F	68F / 77F	"	"	"
DHW Heating	Electric Storage	Electric Heat Pump	"	"	"
DHW Distribution	Standard-flow fixtures	Same as baseline but w/ low-flow fixtures	"	"	"
Lighting	90% high efficacy fixtures	100% high efficacy fixtures	"	"	"
Large Appliances	ENERGY STAR average	Same as Baseline	"	"	"
Miscellaneous Electric Loads	Per DOE Building America Benchmark modeling protocols	Same as Baseline	"	"	"
On-Site PV			6.48 kW system per DU		

DESIGN NARRATIVE

b. Zero Carbon Building Assessment (cont'd)

The following graphs and tables summarize the site energy by end use (Table 2) and HERS Rating

(Table 3) comparing the baseline against the proposed building. Additionally,

(Table 4) shows estimated on-site energy generation for a typical dwelling unit.

Table 2: Site Energy Use Intensity (EUI) by End Use (kBtu/sf-yr) – Typical Duplex Building

	Baseline: IECC 2018	Proposed (ZNC-Ready)	ZNC
Misc. (E)	4.96	4.96	
Vent Fan (E)	2.85	1.61	
Lg. Appl. (E)	3.91	3.57	
Lights (E)	2.05	1.84	
Cooling Fan/Pump (E)	0.12	0.08	
Heating Fan/Pump (E)	0.18	0.08	
Cooling (E)	1.40	1.18	
Heating (E)	7.97	3.75	
Heating, Suppl. (E)	0.10	0.05	
On-Site PV			- 21.04
Total (kBtu/yr)	30.85	21.02	-0.02

**Table 3: HERS Index – Typical Duplex Building
(Per ANSI/RESNET 301-2014 Energy Rating Index Method)**

	Baseline: IECC 2018	Proposed (ZNC- Ready)	ZNC (with PV)
HERS	55	36.3	0.00

Table 4: On-site PV based on PV Watts Analysis – Typical Dwelling Unit

System size (Per dwelling unit)	Estimated Annual Electricity Generation (Per dwelling unit)	Estimated Annual Electricity Consumption (Per dwelling unit)
6.48 kW (18 360W Panels at 0 degree tilt)	7,260 kWh-yr	7,122 kWh-yr

DESIGN NARRATIVE

c. Preliminary LEED Checklist



LEED v4 for Building Design and Construction: Homes and Multifamily Lowrise

Project Checklist

Project Name: Pier 5
Date: 12/24/2020

Y	?	N				
2			Credit	Integrative Process	2	
12	1	0	Location and Transportation			15
Y			Prereq	Floodplain Avoidance	Required	
PERFORMANCE PATH						
			Credit	LEED for Neighborhood Development Location	15	
PRESCRIPTIVE PATH						
8			Credit	Site Selection	8	
1			Credit	Compact Development	3	
2			Credit	Community Resources	2	
1	1		Credit	Access to Transit	2	
3	3	0	Sustainable Sites			7
Y			Prereq	Construction Activity Pollution Prevention	Required	
Y			Prereq	No Invasive Plants	Required	
1	1		Credit	Heat Island Reduction	2	
1	1		Credit	Rainwater Management	3	
1	1		Credit	Non-Toxic Pest Control	2	
7	2	0	Water Efficiency			12
Y			Prereq	Water Metering	Required	
PERFORMANCE PATH						
			Credit	Total Water Use	12	
PRESCRIPTIVE PATH						
3	2		Credit	Indoor Water Use	6	
4			Credit	Outdoor Water Use	4	
21	12	0	Energy and Atmosphere			38
Y			Prereq	Minimum Energy Performance	Required	
Y			Prereq	Energy Metering	Required	
Y			Prereq	Education of the Homeowner, Tenant or Building Manager	Required	
PERFORMANCE PATH						
			Credit	Annual Energy Use	29	
BOTH PATHS						
2	2		Credit	Efficient Hot Water Distribution System	5	
2			Credit	Advanced Utility Tracking	2	
1			Credit	Active Solar Ready Design	1	
1			Credit	HVAC Start-Up Credentialing	1	
PRESCRIPTIVE PATH						
Y			Prereq	Home Size	Required	
	1		Credit	Building Orientation for Passive Solar	3	
	2		Credit	Air Infiltration	2	
2			Credit	Envelope Insulation	2	
	1		Credit	Windows	3	
4			Credit	Space Heating & Cooling Equipment	4	

EA PRESCRIPTIVE PATH (continued)						
3			Credit	Heating & Cooling Distribution Systems		3
1.5	1.5		Credit	Efficient Domestic Hot Water Equipment		3
2			Credit	Lighting		2
2			Credit	High Efficiency Appliances		2
	4		Credit	Renewable Energy		4

8	2	0	Materials and Resources			10
Y			Prereq	Certified Tropical Wood	Required	
Y			Prereq	Durability Management	Required	
1			Credit	Durability Management Verification		1
2	2		Credit	Environmentally Preferable Products		4
3			Credit	Construction Waste Management		3
2			Credit	Material Efficient Framing		2

10	3	0	Indoor Environmental Quality			16
Y			Prereq	Ventilation	Required	
Y			Prereq	Combustion Venting	Required	
Y			Prereq	Garage Pollutant Protection	Required	
Y			Prereq	Radon-Resistant Construction	Required	
Y			Prereq	Air Filtering	Required	
Y			Prereq	Environmental Tobacco Smoke	Required	
Y			Prereq	Compartmentalization	Required	
3			Credit	Enhanced Ventilation		3
1	1		Credit	Contaminant Control		2
1			Credit	Balancing of Heating and Cooling Distribution Systems		3
1			Credit	Enhanced Compartmentalization		1
2			Credit	Enhanced Combustion Venting		2
1			Credit	Enhanced Garage Pollutant Protection		2
1	2		Credit	Low Emitting Products		3

0	2	0	Innovation			6
Y			Prereq	Preliminary Rating	Required	
	2		Credit	Innovation		5
			Credit	LEED AP Homes		1

2	2	0	Regional Priority			4
1			Credit	Regional Priority: Specific Credit		1
1			Credit	Regional Priority: Specific Credit		1
	1		Credit	Regional Priority: Specific Credit		1
	1		Credit	Regional Priority: Specific Credit		1

63	27	0	TOTALS	Possible Points:	110
Certified: 40 to 49 points. Silver: 50 to 59 points. Gold: 60 to 79 points. Platinum: 80 to 110					

DESIGN NARRATIVE

d. Preliminary Boston Climate Resiliency Checklist reflecting proposed outcomes

We have reviewed the December 14, 2017 BPDA checklist. We will file and complete this document prior to submitting under Article 80.

Our basic program is excitement at the waterfront by expanding Charlestown Marina to establish a contemporary and unique floating community. We are making provision for unique floating vessels that will provide a full range of living quarter options in addition to additional small boat dockage. The Pier 5 parcel is located in a Velocity Zone, which means it is vulnerable to storms with a 1% annual recurrence (i.e. 100-year storm) that includes waves greater than 3 feet in height. Our marina engineering will anticipate this, and account for survivability of the facility's infrastructure.

During such a significant storm, occupancy may pose certain challenges which would make evacuations prudent but not essential. Future wave modeling will allow us to project what the wave heights and patterns will be inside the attenuator. The attenuator is a floating breakwater that is engineered for specific applications to mitigate wave action inside controlled waterside environments. Current conceptual plans indicate a 16' wide attenuator, but analysis may suggest increasing this to 20'. The overall marina design is managed first by this component. Its size, width, and depth will help reduce the shorter period waves that enter the basin.

Piles are extended up above the base flood elevation to manage storm surge and rising sea levels, and are designed to anchor the facility in position by resisting lateral loads associated with wind, current, and wave action. The pilings are steel piles coated with epoxy and sleeved with HDPE. All dock framing will be aluminum, and decking will be either composite or IPE in lieu of treated pine, implying strength and resistance to deterioration.

e. Key resilient development

The objectives for energy efficiency are currently being studied. Our intent is to incorporate standards in the live-aboard vessels that approach Passive House and LEED Gold standards. No fossil fuels will be used, and high efficiency heat pumps will be employed for heat, AC, and DHW. It is our intent that Solar PV panels will be integrated into the design, and we are currently evaluating the potential for capturing rain water in hull-sectioned cisterns for grey water, non-potable use as well as using the surrounding water for heat exchange to reduce energy usage within the units. This combined with solar panels could provide a very sustainable solution.

DESIGN NARRATIVE

e. Key resilient development (cont'd)

All extreme weather conditions enumerated in Sections C, D, and E will be reflected in the final design. Rising sea levels are obviously easily mitigated with floatation devices engineered for anticipated increases.

One challenge that requires detailed study is emergency power. Numerous options exist for consideration utilizing marine technology.

Resiliency on land presents challenges integral to working at The Charlestown Navy Yard, identified as a high risk area, that has previously been developed. Our efforts are to weave to the existing infrastructure but create a safe and functional landscape for the public to enjoy. Three basic solutions are being implemented to make this portion of Pier 5 development resilient. First, we plan to use state-of-the-art engineering for all construction of boardwalks and utilities. This will include flexible joinery, isolation, and structural tie-downs. Secondly, we are proposing that the major components of the linear park are all located on the existing seawall, providing a secure foundation for installation. Lastly, we will carefully consider the integrity of all structures in the public realm, which will be designed to either withstand surge, be removable with light equipment, and utilize construction and plant material that is appropriate for this waterside environment.

The public realm at Pier 5 embraces the principles of energy efficiency, water efficiency, and ecological sustainability. Plant species are chosen for drought tolerance to require limited or no irrigation. Stormwater collection, direction, and reuse will be implemented where possible. Lighting will be high-efficiency LED with options for solar and/or wind charging to be explored. Recycled and sustainable materials, local fabrication, and installation will follow Green Building guidelines. The addition of trees to the waterfront is consistent with the principles for heat island mitigation, windbreaks, and revegetation of waterfront edges.

f. Green building strategies

Superstructures and live-aboard vessels will be highly energy efficient, with Passive House being the ultimate goal. The list below is a summary of our green and sustainable goals for the project. See also, attached Zero Carbon Assessment for more detailed information.

1. Envelope improvements, including adding exterior continuous insulation, upgrading to high performance triple-glazed windows, and improving roof/floor insulation greatly reduce heating loads. Additional levels of air tightness beyond code can provide further savings but are not included in this comparison in keeping with modeling conventions.
2. Water heating is a major energy end use, so integrating conservation measures will lead to a significant reduction in energy use. These include upgrading to a heat pump water heater, low-flow fixtures, and efficient pipe distribution runs.

DESIGN NARRATIVE

f. Green building strategies (cont'd)

3. Providing an efficient ERV with a high sensible recovery ratio and low W/cfm fan power rate is important to reduce peak and annual energy loads. We will also be exploring the possibility of water source, hydrothermal technologies for heating and cooling for the vessels while moored.
4. Low lighting power density (LPD) and efficient appliances can further drive down energy usage. For example, high efficiency heat pump dryers can reduce consumption and eliminate venting.
5. All-electric systems and appliances position the proposed design to be zero carbon ready, because of the elimination of on-site combustion and the reduced loads because of the design's efficiency measures.
6. About 6 to 6.5 kW of on-site PV can cover 100% of each proposed dwelling unit's energy usage, on average. This represents about 18 solar PV panels per unit, which can be integrated on the roof of each building either through direct rack-mounting or via integrated canopies.
7. All plantings at the harbor walk will be local and selected to be resistant to salt air and water.
8. Roof water may be stored in the platform hulls and recycled for grey water usage.
9. Moisture resistant materials will be used throughout, and all metals will be stainless steel, coated aluminum, or galvanized steel.

DESIGN DRAWINGS

Included in this section:

1. *A neighborhood plan (at appropriate scale, e.g., 1"=40') as well as a site plan (1" = 20') showing how the proposed design will fit within the immediate context of existing buildings and within the larger context of the neighborhood. The purpose of the neighborhood plan is to illustrate how the project meets the Design Guidelines set forth in this RFP. Therefore, the proposed building(s), existing building footprints, lot lines, streets, street names and any other relevant contextual information should be included in the neighborhood plan. The purpose of the site plan is to illustrate the building footprint and its placement on the site, the general building organization, open space, landscape elements, driveways, curb cuts, fencing, walkways and streetscape improvements. The neighborhood plan and site plan should coordinate through the inclusion of renderings, perspective drawings and aerial views of the project.*
2. *Schematic floor plans (1/8" = 1'-0" scale) showing the basement, ground floor, upper floor(s), and roof, including room dimensions, square footage of rooms, overall building dimensions, and the gross square footage of the building.*
3. *Building Elevations (1/8" = 1'-0" scale) showing all sides of the proposed building, architectural details, building height and notations of proposed materials.*
4. *Street elevations (at appropriate scale, e.g. 1/8"=1'-0") showing the relationships of the proposed building to the massing, building height and architectural style of adjacent buildings. This street context drawing may combine drawings with photographs in any manner that clearly depicts the relationship of the new building to existing buildings.*
5. *Perspective drawings drawn at eye-level and aerial views that show the project in the context of the surrounding area.*
6. *Additional conceptual rendering including Assembly Diagram transition*









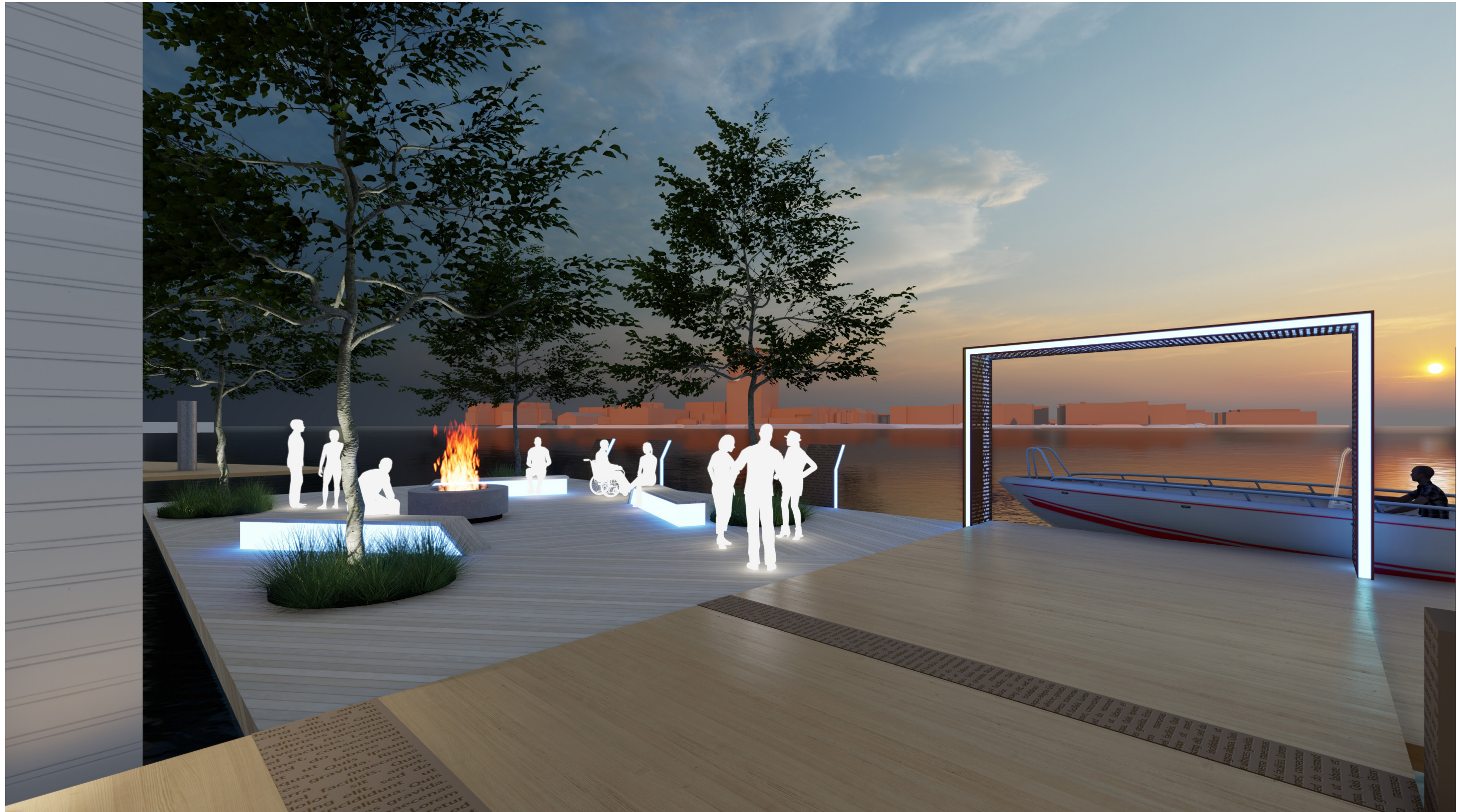










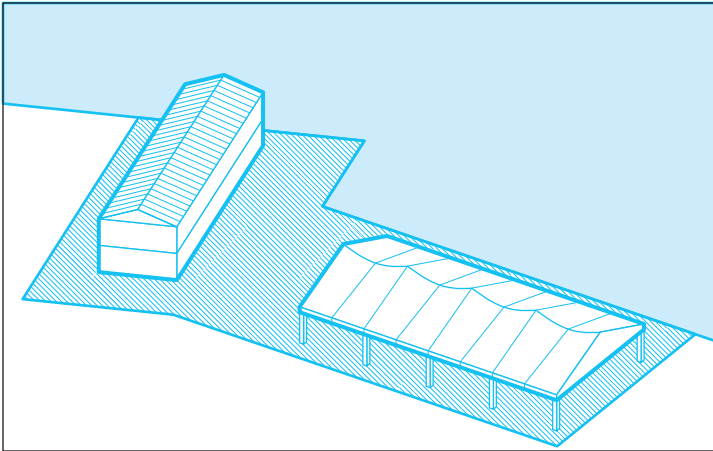




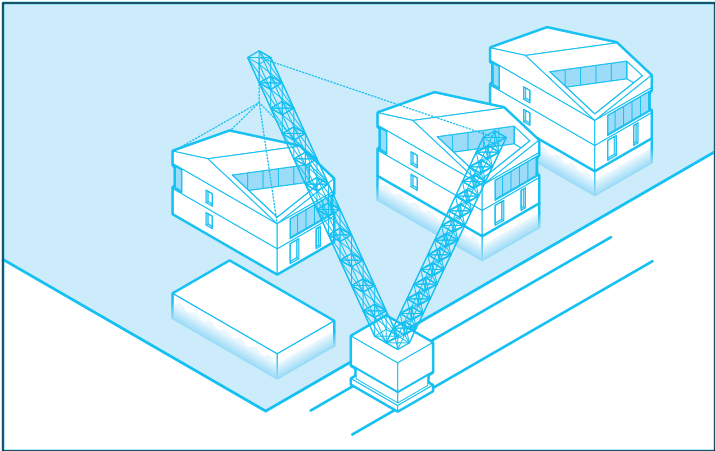


Navy Blue Fabrication and Assembly

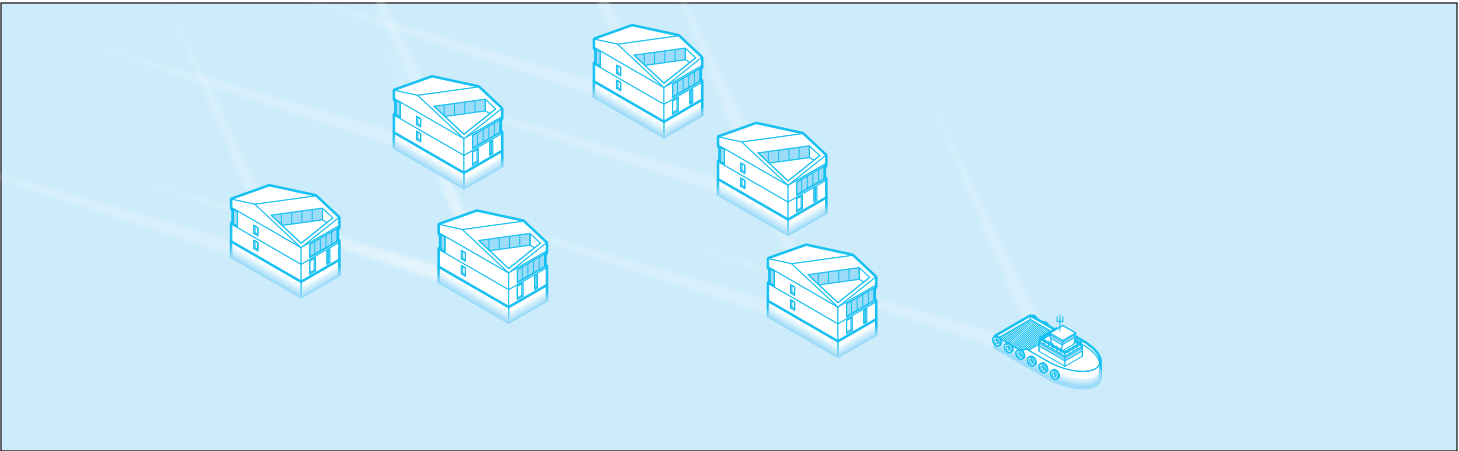
1: Fabricate Modules and Barge Hulls



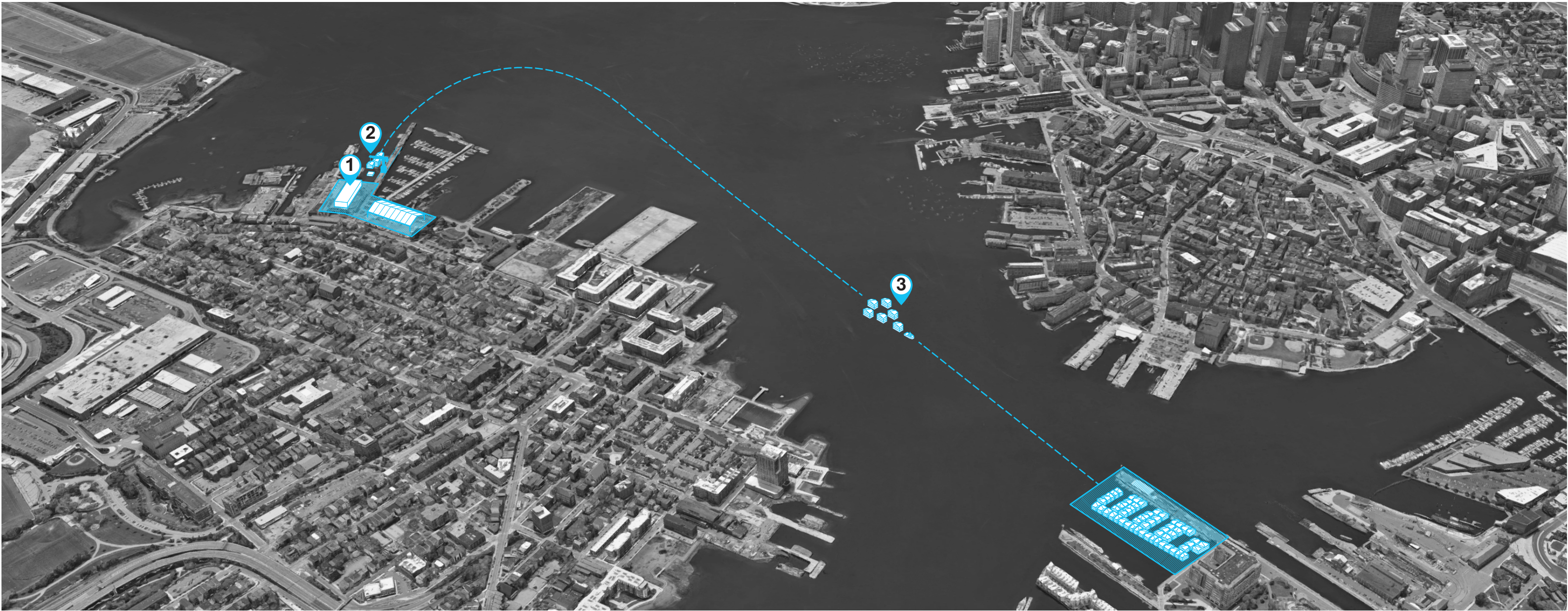
2: Crane Modules onto Barge Hulls



3: Tug Modules to Pier 5



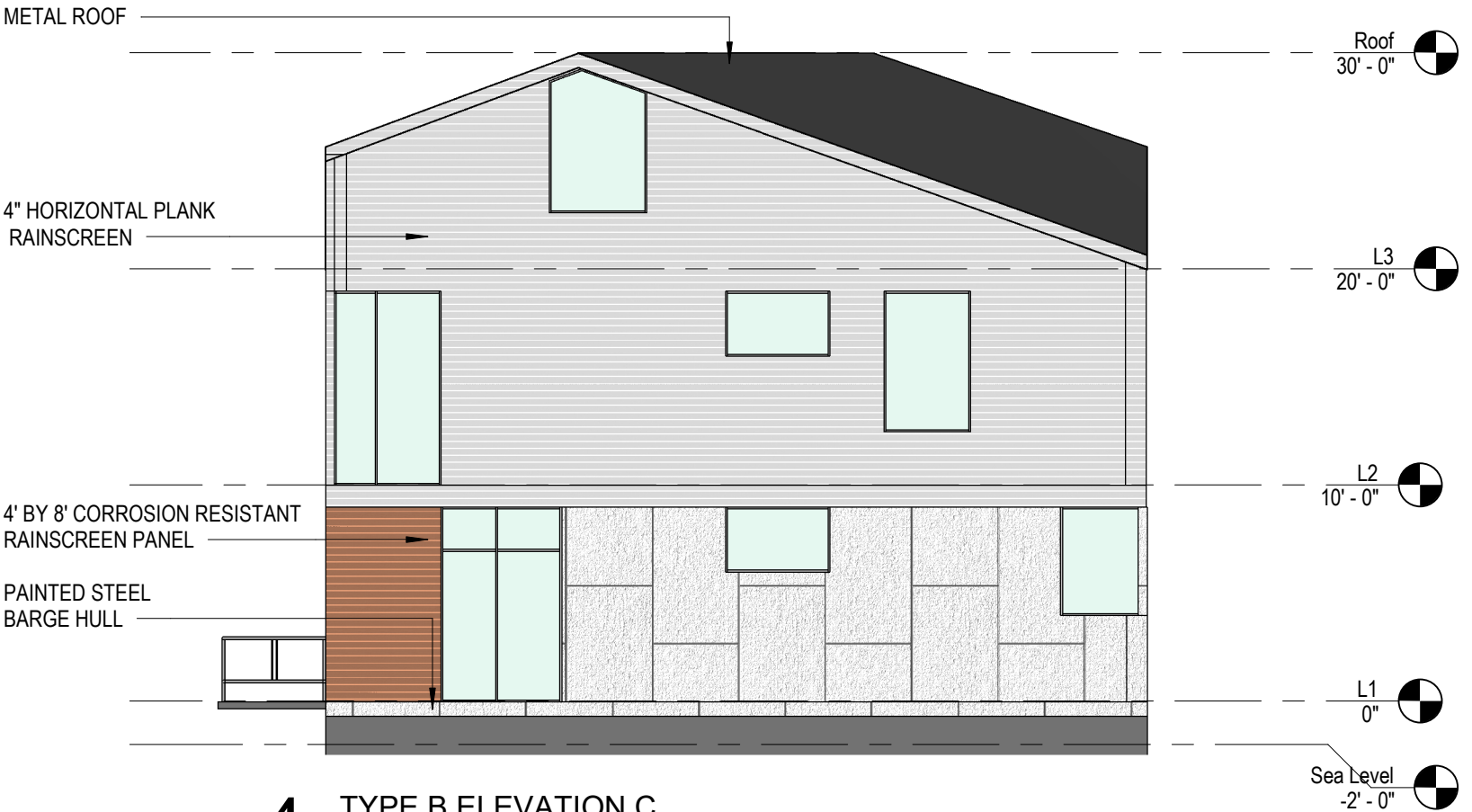
4: Modules Arrive at Pier 5





1 TYPE B ELEVATION A
1/8" = 1'-0"

2 TYPE B ELEVATION B
1/8" = 1'-0"

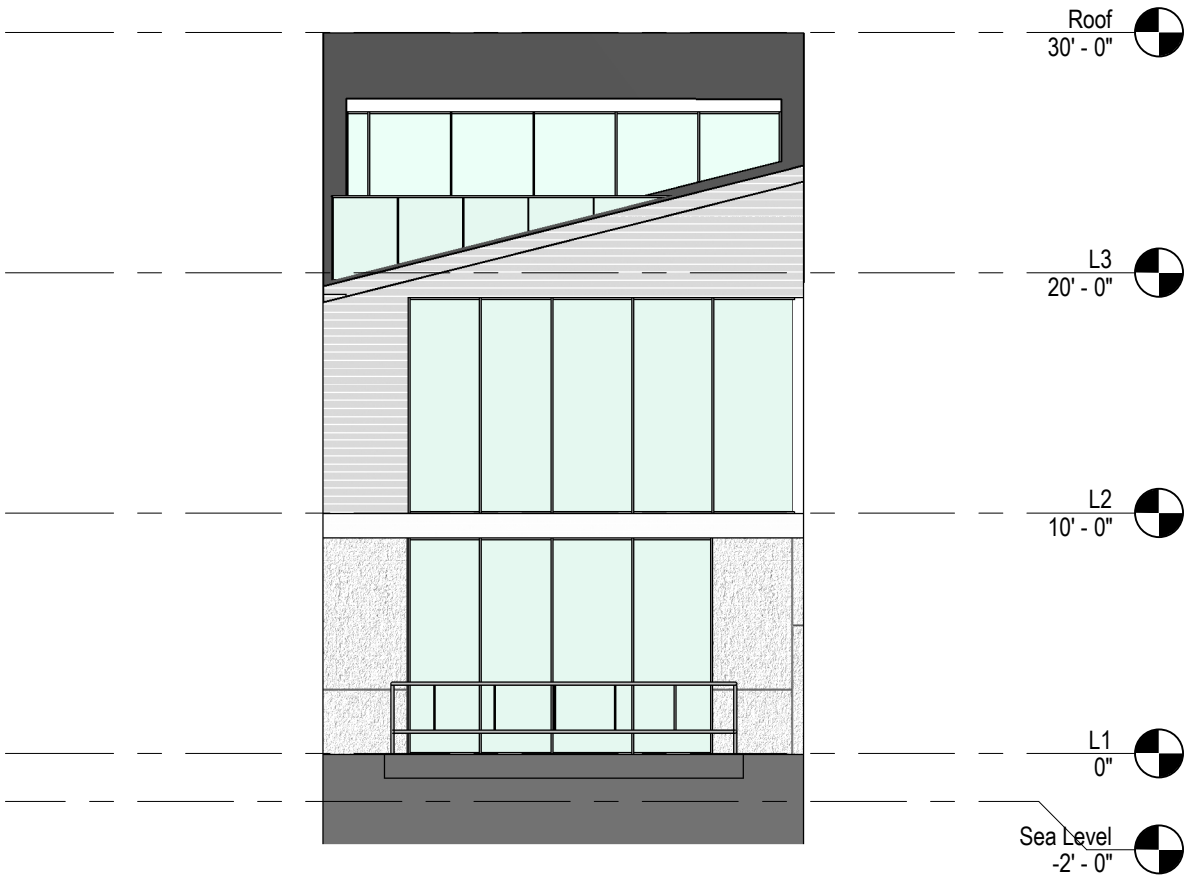


4 TYPE B ELEVATION C
1/8" = 1'-0"

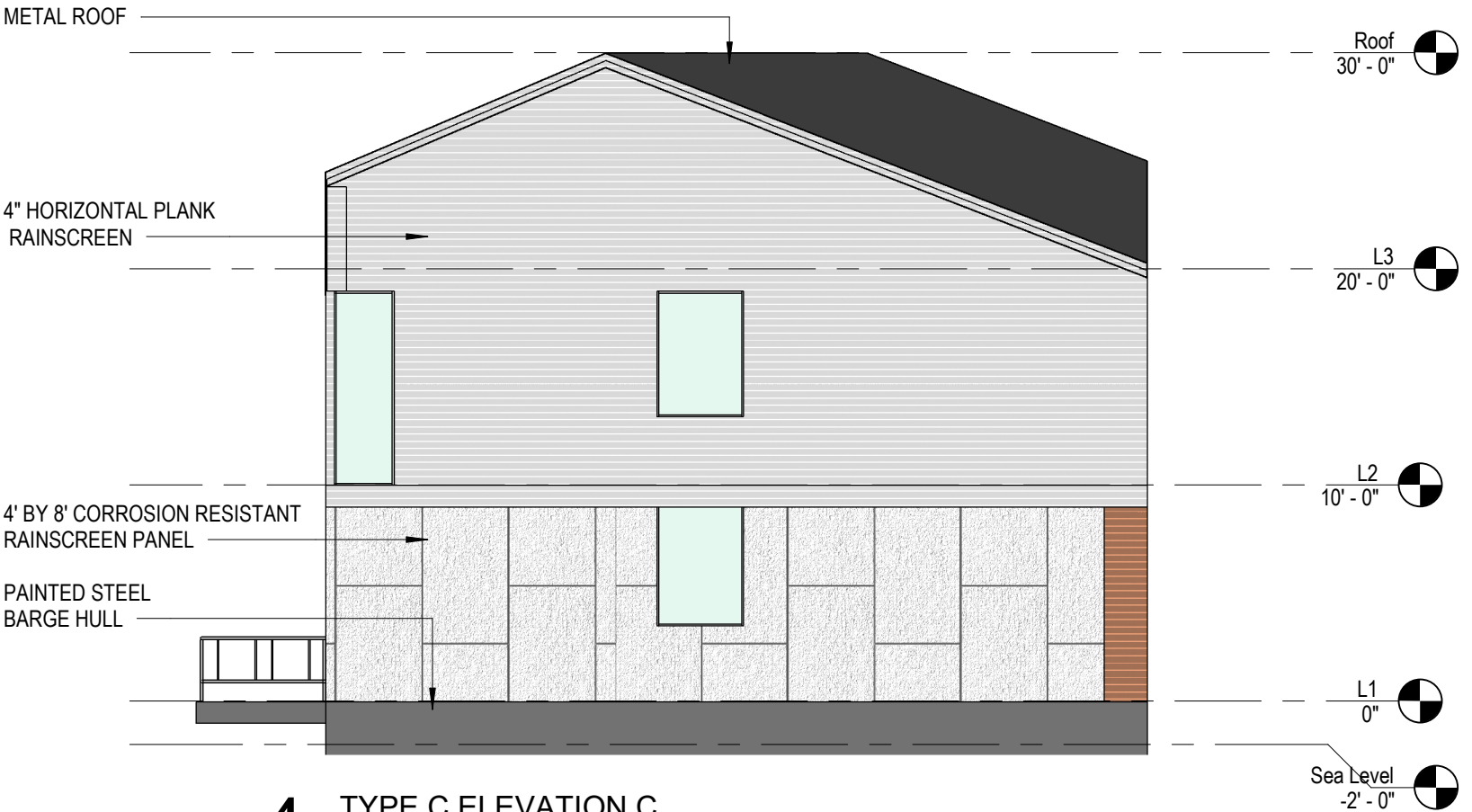
3 TYPE B ELEVATION D
1/8" = 1'-0"



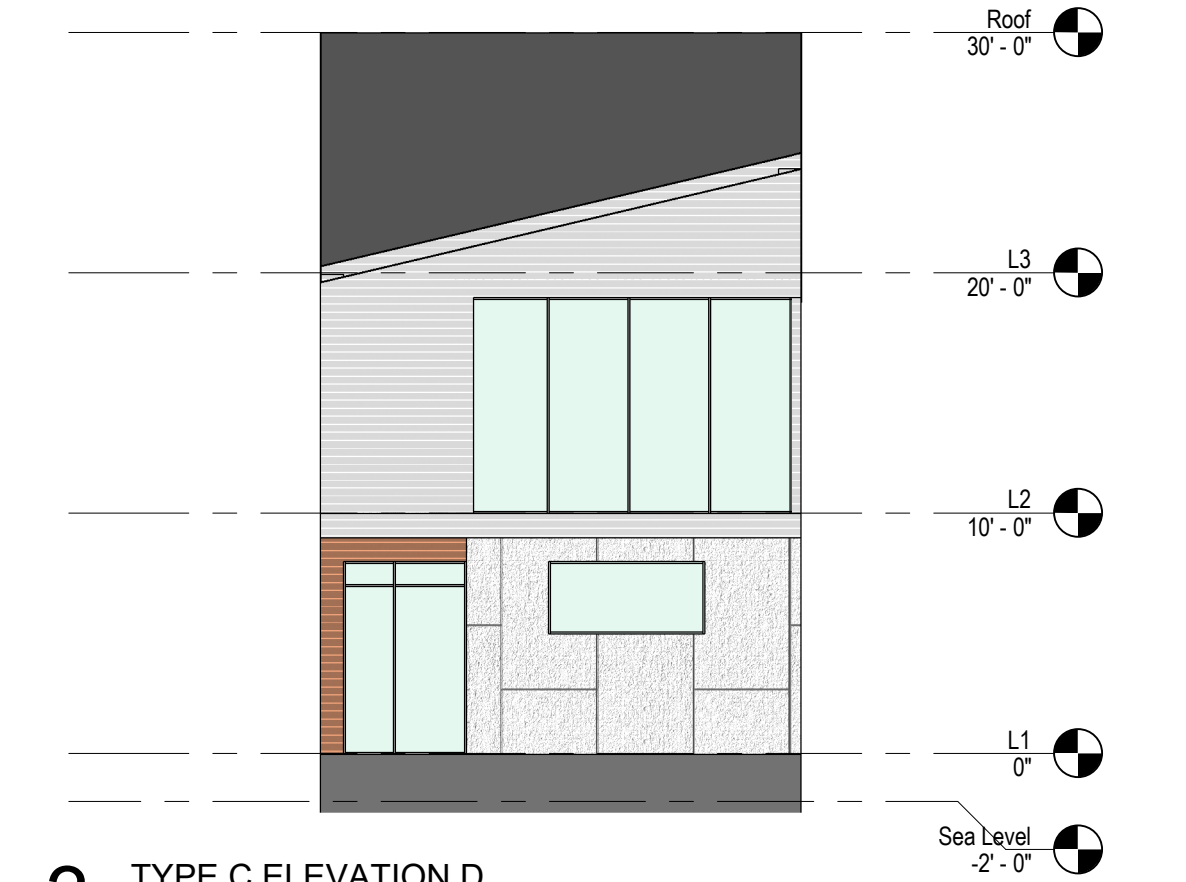
1 TYPE C ELEVATION A
1/8" = 1'-0"



2 TYPE C ELEVATION B
1/8" = 1'-0"



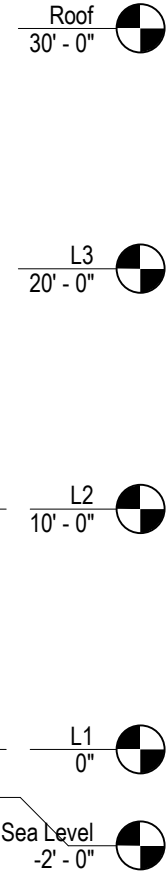
4 TYPE C ELEVATION C
1/8" = 1'-0"



3 TYPE C ELEVATION D
1/8" = 1'-0"



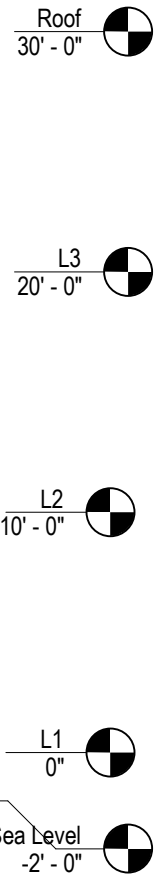
1 TYPE F ELEVATION A
1/8" = 1'-0"



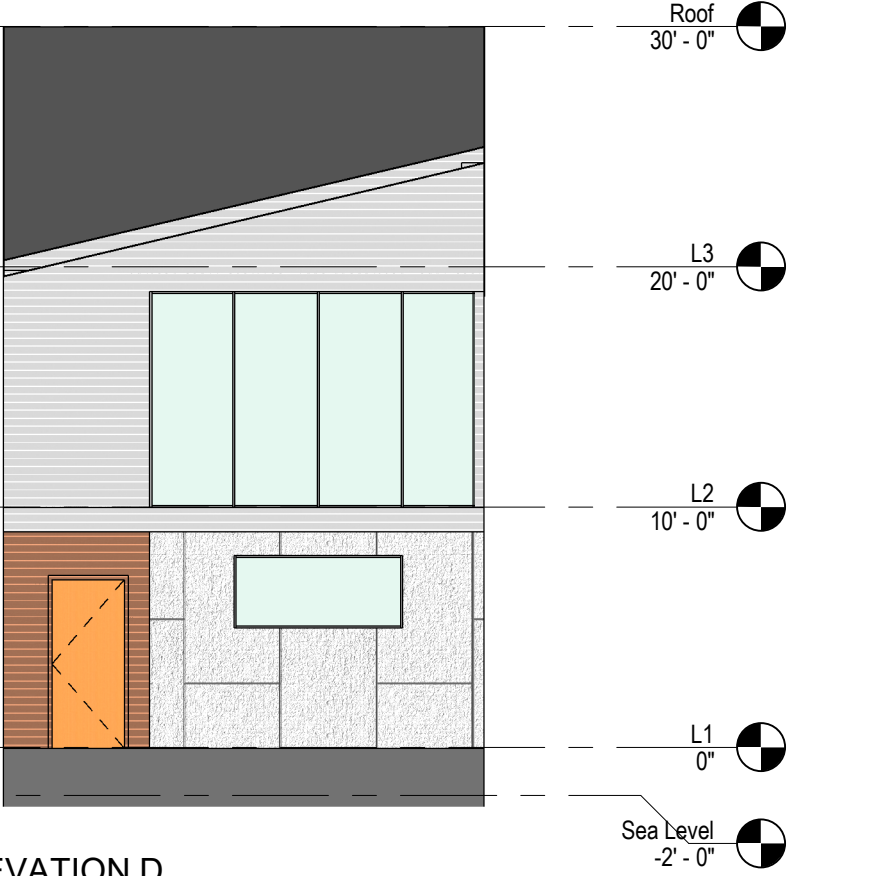
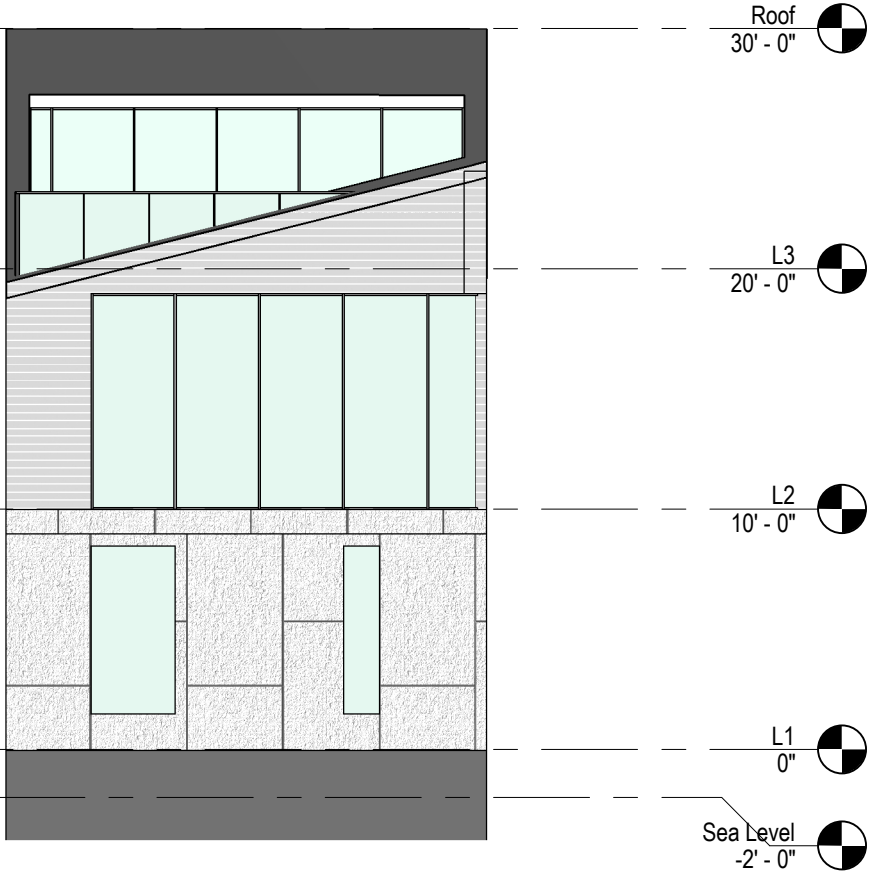
2 TYPE F ELEVATION B
1/8" = 1'-0"



4 TYPE F ELEVATION C
1/8" = 1'-0"

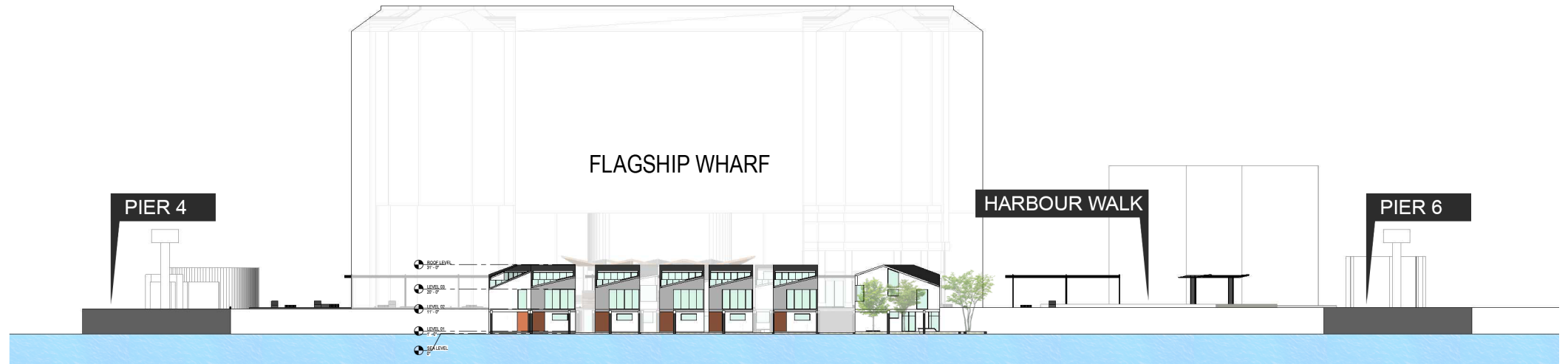


3 TYPE F ELEVATION D
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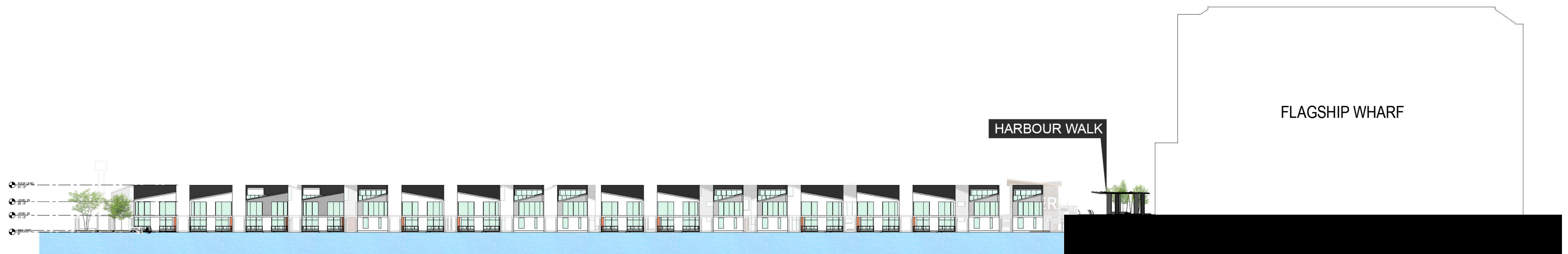




NORTH ELEVATION



SOUTH ELEVATION



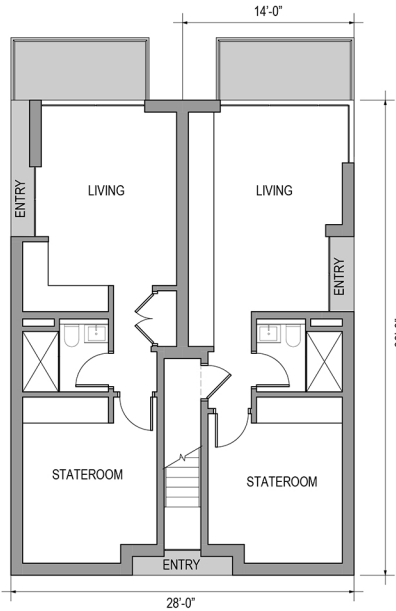
EAST ELEVATION



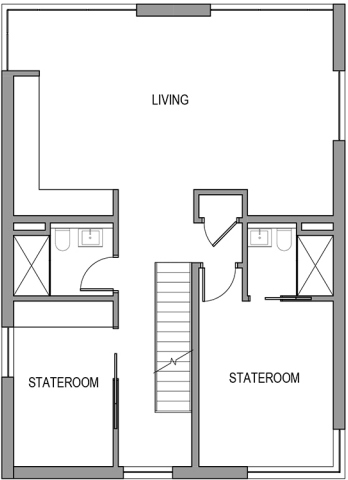
WEST ELEVATION

TYPE B - LIVE-ABOARD VESSEL

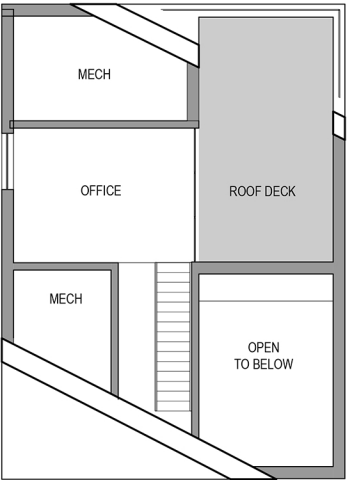
12 HULLS - 36 LVs
(2) 1 STATEROOM - 530 GSF
(1) 2 STATEROOMS - 1210 GSF



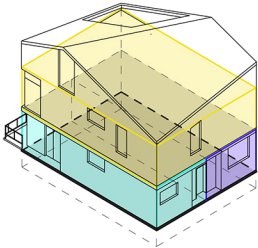
LEVEL 1 PLAN
SCALE: 1/8" = 1'-0"



LEVEL 2 PLAN
SCALE: 1/8" = 1'-0"

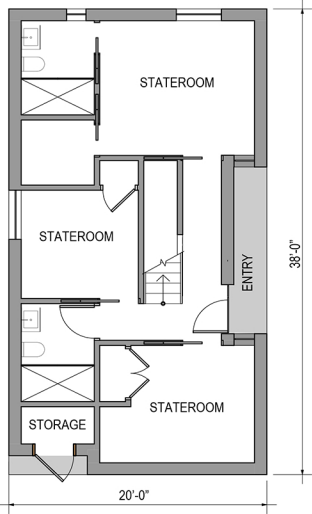


ROOF DECK
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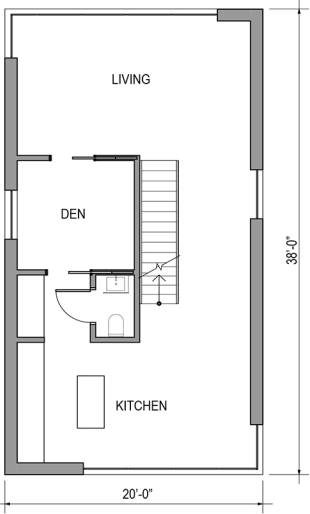


TYPE G - LIVE-ABOARD VESSEL

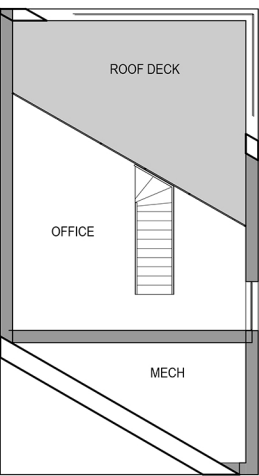
3 HULLS - 3 LVs
(1) 3 STATEROOMS - 2,100 GSF



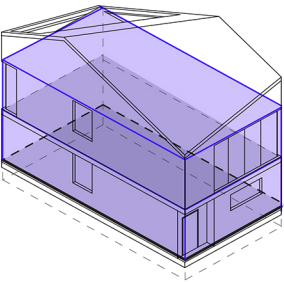
LEVEL 1 PLAN
SCALE: 1/8" = 1'-0"



LEVEL 2 PLAN
SCALE: 1/8" = 1'-0"

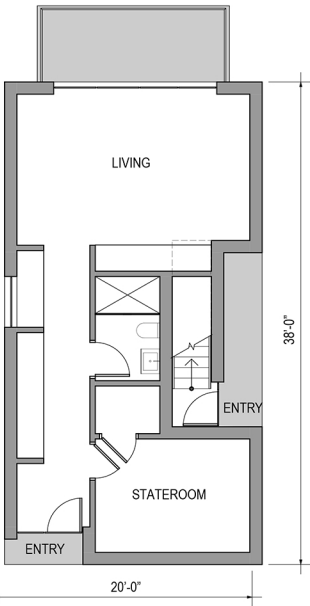


ROOF DECK
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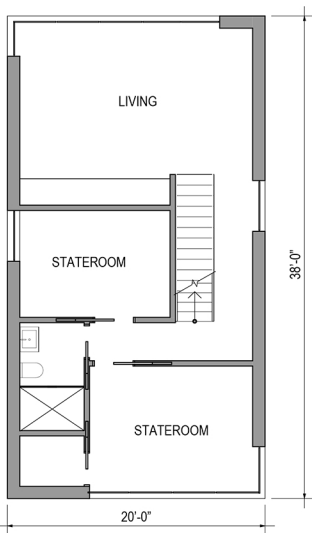


TYPE E - LIVE-ABOARD VESSEL

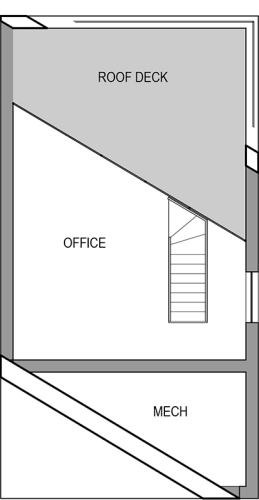
11 HULLS - 22 LVs
(2) 1.5 STATEROOMS - 900 GSF



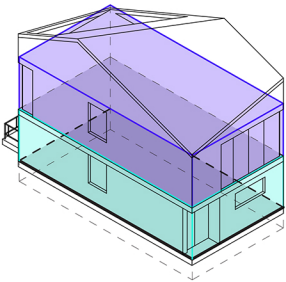
LEVEL 1 PLAN
SCALE: 1/8" = 1'-0"



LEVEL 2 PLAN
SCALE: 1/8" = 1'-0"



ROOF DECK
SCALE: 1/8" = 1'-0"



Team Profiles



617-242-2020
charlestownmamarina.com

Ann Lagasse and Chuck Lagasse
Principals, Charlestown Marina / Ocean Havens LLC

In 2007 Ann and Chuck founded Ocean Havens LLC a real estate company that focuses on acquisition, development and management of distinct waterfront properties, marinas and uplands. The company has created exciting destinations for boating in New England including Boston Yacht Haven Inn & Marina, Charlestown Marina, Fan Pier Marina, Boston Harbor Shipyard & Marina, and Provincetown Marina.

Prior to founding Ocean Havens LLC, Ann and Chuck spent 25 years acquiring, redeveloping and managing buildings and marinas in downtown Newburyport Massachusetts. The company's focus was adaptive reuse of historic buildings and a creative mix of tenants. They received numerous awards for their historic preservation and beautification work. Chuck also founded and ran Plastek Corp. for 30 years, a multi process plastics molding company with 500 employees nationally. The company was public for 10 years, private for 20 years and sold in 2008.

Ann serves as chairwoman of the Board of Trustees at Joslin Diabetes Center and is a board member of Boston Harbor Now. Ann graduated from Regis College and received her M.B.A. degree from Northeastern University. Chuck serves on the board of the Center for Coastal Studies, Provincetown, MA. He has a degree in engineering from University of Lowell.

Working with Spaulding Rehabilitation Hospital, Ann & Chuck host the Impossible Dream Sailboat at the Charlestown Marina. The Impossible Dream is a sailing program focused on improving the lives of participants living with disabilities through sailing.

Related projects

- Boston Yacht Haven Inn & Marina
- Charlestown Marina
- Fan Pier Marina
- Boston Shipyard and Marina





www.urbanspacesllc.com

Founded in 2004, Urban Spaces is an award-winning Cambridge-based real estate development company that aspires to have a transformational effect on the communities it serves. Urban Spaces is committed to delivering successful mid-sized residential and commercial projects in up-and-coming neighborhoods which are in close proximity to public transportation, universities, hospitals, and employment hubs.

STRATEGIC FOCUS

We select projects in downtown settings with unique characteristics.

ENTREPRENEURIAL APPROACH

We are creative, flexible, and tenacious in order to complete complex deals.

EXPERIENCED TEAM

We have the knowledge and sophistication to achieve success in urban development.

EXCEPTIONAL RETURNS

We achieve standout results due to our focus, approach, and experience.



JACK GILES

Chairman

Jack is a partner at Urban Spaces and is a board member of several other businesses throughout the country. He is an active real estate investor with projects in Cambridge/Boston, Dallas, and San Diego. He is also a member of the Advisory Board for the College of Engineering at the University of Arkansas, his alma mater.



PAUL OGNIBENE

Chief Executive Officer

Paul sets the vision and direction for the company and provides executive oversight of all client relationships, contract negotiations, and multi-million dollar budgets. Paul earned his BA in Economics from Boston College and an MBA from Harvard Business School. Paul and his wife and children have lived in Cohasset for twelve years.



DAVE NOTTER

Chief Operating Officer

Dave provides executive oversight of all project management and asset management activities. Dave develops and implements infrastructure, processes, and policies to help the company grow. Dave earned a BS in Finance from Boston College and an MBA from The University of North Carolina, Chapel Hill.



JEFF HIRSCH, R.A.

Vice President of Operations

Jeff oversees the day-to-day operations of all project activities, including permitting, design, and construction. With over 25 years of industry experience, Jeff is both a licensed architect and general contractor and earned a BA in Environmental Design and a Masters of Architecture from Texas A & M University



SKIP ROSE, P.E.

Director of Construction and Facilities

Skip coordinates project management activities and provides operational expertise. As a licensed Professional Engineer, Skip has more than 25 years of hands-on project management and systems development experience. Skip earned a degree in Physics from Binghamton University.



KATIE ESTES

Asset Manager

Katie performs financial analysis of potential projects and manages all asset management and project marketing activities. Katie serves as the company's liaison between third-party property management and marketing firms. Katie earned her BA from Tufts University and is currently taking classes in Real Estate Finance at Boston University.



JESSE RUGGIERO

Director of Accounting

Jesse handles all project accounting and cash management, including construction requisitions and lender draws. Jesse earned a BA in economics from Connecticut College.



ANDREA DOCANTO

Executive Assistant and Office Manager

Andrea is responsible for supporting all office operations. Andrea is currently earning her Entrepreneurial Science degree from Bunker Hill.



PARENT+DIAMOND

REAL ESTATE DEVELOPMENT

RALPH PARENT

Ralph Parent attended Boston College and, among many other activities, was a four-year defensive starter for Boston College football team. Upon graduation, he coached football at both Brookline High School and Everett High School.

Always interested in real estate, he began a mentor-mentee relationship with current business partner, Merrill Diamond, that resulted in his complete immersion in the field. As a real estate agent with Blueprint Brokers in Everett, MA, he learned about his chosen profession and soon became a Real Estate Broker creating Parent Realty Advisors. It wasn't long before his entrepreneurial spirit led to founding Pederson Consulting (MBE). Pederson Consulting eventually became one of the key members of a number of development teams who wanted to draw upon his experience and talent working with Boston's Inspectional Services Department regarding permits and approvals. His activities in this regard include coordinating and managing all activities related to securing approvals from Boston's Zoning Board of Appeals and Building Permits.

More recently, Ralph Parent and Merrill Diamond, in conjunction with Urban Spaces, created a company called The Boston Partnership For Community Reinvestment, LLC, ("BPCR") in order to partner with Boston City - Department of Neighborhood Development ("DND") in the creation of affordable housing on scattered sites in Dorchester, Roxbury, and Mattapan. His efforts proved to be the key to sourcing local, disadvantaged General Contractors, Subcontractors, and Venders in order to comply with DND's requirements for a socially responsible approach to this type of development, one focused on Diversity and Inclusion.

The above led to the creation of Parent+Diamond, LLC, a Dorchester-based, MBE pending, real estate development company. Its mission is to bring the type of Diversity and Inclusion that is more frequently mandated in the public sector to our private sector work. Based on a strategic alliance with Urban Spaces, a company with whom Diamond had an ongoing relationship and development projects, Parent+Diamond, LLC has the operational and financial foundation to accomplish this mission, the basis of which is shared by Urban Spaces.

MERRILL H. DIAMOND

Merrill H. Diamond is a founding partner of Diamond Sinacori, LLC, a Boston-based Real Estate Development company founded in 1978.

Merrill is the recipient of numerous national awards for environmentally sensitive design, historic preservation, adaptive re-use, and creative business development. He has served as both a gubernatorial appointee to the Massachusetts Historical Commission and to the Senate Special Commission on Historic Preservation. He is a recent member of the Board of Directors of Preservation Mass, the Commonwealth's statewide, non-profit preservation agency. In addition, Mr. Diamond has been named "Entrepreneur of the Year" by Arthur Young / "Venture Magazine;" "Merchant Builder of the Year" by the National Association of Homebuilders (NAHB), and one of "America's Most Valuable People" by "USA Today," the nation's national newspaper. He has been profiled in Jeffrey L. Seglin's book, America's New Breed of Entrepreneurs, and his development firm has twice been named "One of America's Fastest Growing Companies" by "Inc. Magazine."

Mr. Diamond's recent private-sector real estate development projects include the following: Stoneleigh, the historic preservation and revitalization, the Old Norfolk County Jail in Dedham, MA into residential condominiums; Kendall Crescent, adaptive-reuse, historic preservation, and new construction of the historic Public Works Complex, including the historic Sewell School and Old Town Barn in Brookline, MA into residential condominiums and office space; The Waterworks at Chestnut Hill, a 112 unit residential condominium complex and museum developed in a JV with EAF and overlooking the Chestnut Hill Reservoir in Chestnut Hill, MA; and The Shops at OCEAN'S GATE, A 40,000 sf retail center that is the new "face" of the downtown in Marshfield, MA. The firm recently developed The Lancaster, a JV development with Urban Spaces of 55 new condominium units in Boston, MA and the historic Wild-Sargent Estate in Brookline. Currently, the firm is developing 10 condominiums in the historic Kelley School in Newburyport; 61 condominiums in downtown Salem and 13 residences on the historic Richardson Estate overlooking the Brookline Reservoir; both in Joint Venture with Urban Spaces.

Mr. Diamond has also done work in the public sector under the auspices of HUD's innovative HOPE VI program. In addition to working on the revitalization of a number of distressed communities across the country, Mr. Diamond is credited with creating and implementing "The Indianapolis Approach," a method of utilizing local disadvantaged contractors to construct HOPE VI developments -- thus turning HOPE VI construction dollars into an economic development generator to: a) build capacity for local businesses; b) create sustaining jobs; and c) keep most of the HOPE VI grant re-circulating within the impacted community. Mr. Diamond has spoken at a number of national symposiums relative to this approach and his views on public and affordable housing have been widely published.



THE PARENT+DIAMOND STORY

Ralph Parent's relationship with Merrill Diamond dates back 30 years, starting with Diamond's coaching Parent in Little League baseball when he was 10. Since that time, Mr. Diamond has continued the mentoring relationship throughout Mr. Parent's academic and professional career.

More recently, Ralph Parent became a real estate agent. Concurrently, he had been in partnerships with other real estate developers to develop single and two-family homes in the City of Boston.

With the retirement of Merrill Diamond's longtime partner in Diamond Sinacori, Mr. Parent and Mr. Diamond have agreed to join forces and rebrand Diamond Sinacori, LLC as Parent+Diamond, LLC, ("P+D") in order to continue doing socially responsible development work in the private sector as well as continuing such work in the public sector.

Both Parent and Diamond have previously developed real estate projects, but the principals differ in their respective experience and scale of their projects. However, for P+D, most of the principal responsibilities will overlap, with Ralph Parent being the majority owner and thus the decision-maker for the partnership. All of our work occurs under the umbrella of P+D's mission to bring Diversity & Inclusion strategies to the private sector. This includes sourcing land or buildings for projects, project management, and the oversight of projects from start to finish.

Ralph Parent will provide a special focus and responsibility regarding the management of the entitlement process and, at the completion of construction, working with the marketing and sales team. Merrill Diamond's primary focus will include, with Ralph Parent's participation and input, deal negotiations, development management, and the identification of appropriate funding sources for Parent+Diamond's ongoing development projects.



Stephen E. Tise Sr. AIA, NCARB
Principal

TISE DESIGN ASSOCIATES
Comprehensive Design Solutions



ARCHITECTURE | PLANNING | PROGRAM MANAGEMENT

Stephen Tise AIA, who will manage the Navy Blue design group, has spent his entire career in the design and construction of large urban initiatives, with an emphasis on utilizing disadvantaged business enterprises. For 13 years, Mr. Tise work with the late Don Stull, a leading American black architect. Steve's primary responsibility was marketing a black design firm when there were none, and monitoring DBE procurement opportunities through State and Federal (8A) initiatives. By the early 1970's, Stull Associates grew to 55 employees and enjoyed an admirable reputation. Steve was also instrumental in helping start Boston's first black owned bank, Unity Bank in Roxbury.

Subsequent to this, Mr. Tise started his own design firm in the mid-1970s. By the mid-1980s, Tise Design had grown to 60 people and provided full service design to non-profit sponsors and public housing authorities. This project type required significant utilization of disadvantaged businesses in both design and construction, and the firm was known regionally for its expertise in community oriented design.

As a result of his reputation and experience, Mr. Tise was asked by The Council of Large Housing Authorities (CLPHA) to represent the organization in working with HUD to develop an innovative new urban strategy called HOPE VI. This initiative emerged from the Senate study on distressed public housing and became the first truly holistic approach to urban disinvestment in blighted communities. Mr. Tise assisted HUD in shaping a program that went well beyond real estate and focused on job training, DBE utilization in host communities, education, social services, credit enhancement and many other pertinent issues. One of Mr. Tise's primary functions with HUD was training resident groups in how to benefit from and participate in these urban transformations (Section 3).

After spending two years consulting with HUD, Mr. Tise worked with a Boston based team to realize HOPE VI success in many eastern cities, including Boston, Cambridge, Hartford, New Haven, New York, Indianapolis, Detroit, Houston and Biloxi, Mississippi. A focus of all these projects was strengthening minority communities by providing services and opportunities for participation. HOPE VI has been described by HUD as the most successful urban initiative ever undertaken in the United States.

In addition, Mr. Tise was a Boston Society of Architects Scholar, a Loeb Fellow at Harvard, and a National Endowment of the Arts Fellow. He served on the Governor's Housing Advisory Commission under Michael Dukakis and currently serves on the Newton Housing Partnership.



Stephen E. Tise Sr. AIA
Principal

REGISTRATION

Massachusetts #4184
Connecticut #ARI.0008400
NCARB Certificate #42372
(Active registration in Rhode Island)

PROFESSIONAL EDUCATION

Harvard University, Loeb Fellow, Cambridge, Massachusetts 1978
Boston Architectural College, Boston, Massachusetts 1970

AFFILIATIONS

American Institute of Architects
Boston Society of Architects
Congress on New Urbanism
National Association of Housing and Redevelopment Officials
U.S. Green Building Council

PROFESSIONAL EXPERIENCE

PUBLIC HOUSING PLANNING & DESIGN

Miller's River/LBJ	Cambridge, MA
Commonwealth Development	Boston, MA
Orient Heights Redevelopment	Boston, MA
Heath Street Redevelopment	Boston, MA
William Woods Development	Providence, MA
Putnam Gardens	Cambridge, MA
Roosevelt Towers	Cambridge, MA
Eastside	New Haven, CT
Monterey Place	New Haven, CT
Old Colony	Boston, MA
Riverside Apartments	Ansonia, CT
Thames River Apartments	New London, CT
Pleasant St. Apartments	Marlborough, MA
Farnam Courts Phase I	New Haven, CT
Farnam Courts Phase II	New Haven, CT
C.B. Motley (R.A.D.)	New Haven, CT
Waverly (R.A.D.)	New Haven, CT
St. Botolph's (R.A.D.)	Boston, MA
Wellesley Housing Authority	Wellesley, MA
Worcester Housing Authority	Worcester, MA

202 HOUSING

Casa Maria Housing	Boston, MA
Mill Pond Apartments & Group Residence	Littleton, MA
Nate Smith House	Jamaica Plain, MA
Julia Martin House	Jamaica Plain, MA

RESIDENTIAL

Southwick Block Apartments	Lowell, MA
Salem 667 Elderly Housing	Salem, MA
Marshfield Congregate Housing	Marshfield, MA
Summit Place Condominium	Norwood, MA
Cambridgeport Common	Cambridge, MA
Villa Ladine	St. Barthelemy, FWI
Les Etoiles	St. Barthelemy, FWI
Beechtree Place	Brookline, MA
Dorchester Bay Modular Housing	Boston, MA
Winchester Place Condominium	Brookline, MA
Warren Street Senior Housing	Watertown, MA
100 Centre Plaza	Brookline, MA
Carol Ave. Accessibility Adaptation	Brighton, MA
Egmont & High St. Development	Brookline, MA
Franklin Field South Family Apartments	Boston, MA
Miller's River/LB Johnson/HS Truman	Cambridge, MA
Windsor Village	Waltham, MA
Pine Manor College	Brookline, MA
Davis Avenue Condominiums	Brookline, MA
Guild Row	Roxbury, MA
Fenway Renovations	Boston, MA
Plainville DMR Housing	Plainville, MA
Loring Towers	Salem, MA
The Lancaster	Boston, MA
1650 Commonwealth	Boston, MA
65 Washington Street	Salem, MA

RESIDENTIAL (cont'd)

Dummer Street	
Brookline, MA	
75 Amory Street	Boston, MA
The Kelley School	Newburyport, MA
Bigelow Street	Brighton, MA
Richardson Estate	Brookline, MA
198 Harvard St.	Brookline, MA
950 Falmouth Rd. Affordable	Mashpee, MA



utile

ABOUT UTILE

Utile is a Boston-based design firm built like a think tank. We thrive on solving complex problems in intelligent and pragmatic ways. From theoretical issues that frame policy to the practical implementation of architectural commissions, Utile develops a rigorous research-based approach for finding the best solutions.



Design Process

The firm's work is not only driven by aesthetics, but by a shared interest in doing deep-dive research focused on the cultural, social, regulatory, and environmental issues inherent in a given design problem. Utile finds opportunities for design by uncovering these issues and synthesizing them.

As a result, Utile doesn't focus on a particular project type, but rather projects that require comprehensive research and collaborative engagement with their clients. This means that the firm works on diverse projects of varying scales.

Utile has charted a new practice model that combines the innovative thinking of a design practice with the proactive strategic insights of business consultants. As the design process unfolds, the firm uncovers hidden correspondences and tests potential conceptual approaches through sophisticated information graphics, maps, and drawings.

Design Expression

The expression of Utile's projects results from considering the visual and functional role of building components—the roof, exterior cladding, windows, and doors. The firm champions the idea of the “multifunctioning element” and avoids adding ornamentation to create purely visual effects. The joints, drip edges, frames, and other details necessitated by well-informed construction practices are leveraged for maximum effect in an overall expressive approach. This larger composition carefully considers the scale and materials of neighboring buildings and finds the appropriate expression given the purpose of the project. The firm distinguishes between the role of background buildings and civic buildings—even in its own design proposals. Utile frames their projects within the continuum of a constantly evolving urban fabric and sees the thoughtful evolution of the city—more than the design of eye-catching one-off proposals—as the firm's primary focus.

Name and Address

Utile, Inc.
115 Kingston Street
Boston, MA 02111
Date Established

2002

Company Type

Corporation

Management Team

Tim Love, Principal and President

Michael LeBlanc, Secretary and Principal
Matthew Littell, Treasurer and Principal

Mimi Love, Principal

Employees

67 total (22 licensed architects, 59 professional design staff, 6 admin, 2 interns)

Major Areas of Focus

Urban design, urban planning, architecture



Michael LeBlanc AIA
Principal-in-Charge

Michael is a principal at Utile, where he has been in charge of numerous residential, institutional, and renovation projects such as the 160-unit mixed-use Girard in Boston's South End and The Quinn mixed-use development at 380 Harrison Avenue in the South End. He managed the Rethink Boston City Hall & Plaza Master Plan and the addition to and renovation of the Jamaica Plain Branch of the Boston Public Library. His interests range from the logics of construction and material research, to investigating the parametric relationships between code, cost, sustainability, and experiential qualities in building design.

Prior to Utile, Michael worked at Machado Silvetti where he was a senior designer and project director on projects such as the Provincetown Art Association Museum (PAAM) addition and renovation, which became the first LEED-certified art museum in the United States; the Honan-Allston Branch of the Boston Public Library, which won a 2003 AIA National Design Award; and the Getty Villa, Research Center, and Scholars' Library.

Additionally, Michael has taught design studios as a Faculty Associate at Arizona State University and is currently a member of the adjunct faculty at Northeastern University's Department of Architecture. He is also on the Board of Directors for the Boston Preservation Alliance.

Education

Arizona State University, Master of Architecture, 1997
University of Massachusetts, Bachelor of Fine Arts, Architectural Studies, 1991
Affiliations
American Institute of Architects
Boston Preservation Alliance (Board of Directors)
Northeastern University, Department of Architecture, Lecturer, Adjunct faculty, 2001-present
Arizona State University, Department of Architecture, Adjunct faculty, 1997-1998

Selected Projects

Girard, Boston, MA—160-unit mixed-use development for New Atlantic
The Quinn, Boston, MA—14-story, 356,500 SF mixed-use building for Related Beal
26 West Broadway, South Boston, MA—31-unit mixed-use development for The Cincotta Companies
The Neponset Condominiums, Quincy, MA—for Cuttyhunk Capital
The Visionary Boutique Hotel, South Boston, MA—for The Cincotta Companies
Front Street Development Design, Portland, ME—85-units for the Portland Housing Authority
E+ Housing, Boston, MA—Two zero energy townhouses
7 East Springfield Street Condominiums, Boston, MA
Cambridge Street Mixed-Use Development, Cambridge, MA
Residences at 557–559 East Second Street, South Boston, MA
Webster Block, Chelsea, MA—141 units of multifamily housing
375 Broadway, Chelsea, MA—Residential conversion
Rethink Boston City Hall and Plaza Master Plan, Boston, MA
Dudley Branch of the Boston Public Library Renovation, Roxbury, MA
Dudley Branch of the Boston Public Library Programming Study and New Entry Design, Roxbury, MA
Jamaica Plain Branch of the Boston Public Library Renovation & Addition, Jamaica Plain, MA
ZUMIX, East Boston, MA—LEED Gold certified historic renovation for a non-profits arts organization
Tufts University Junior-Senior Housing Study, Somerville, MA
Scape Davis Square Student Housing, Somerville, MA
Boston Harbor Islands Pavilion, Boston, MA—for National Park Service & Harbor Island Alliance
The Harbor Hotel Repositioning, Provincetown, MA
Virginia Commonwealth University School of Art and Design, Qatar Foundation
Hassayampa Academic Village, Arizona State University, Tempe, AZ—while with Machado Silvetti
Honan-Allston Branch of the Boston Public Library, Allston, MA—while with Machado Silvetti
Provincetown Art Association Museum (PAAM), Provincetown, MA—while with Machado Silvetti

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Brendan Willis AIA
Project Manager

Brendan Willis joined Utile in 2018. He de-signed the renovation of amenity spaces at Troy Boston and lobby and residential ame-nities at One India Street in Boston, both for Equity Residential. He is currently on the design team for a boutique hotel at 248 Dorchester Avenue in South Boston for The Cincotta Companies as well as a new mixed-use development on Cambridge Street in Inman Square for Boston Nightlife Ventures.

Prior to moving to Boston, he worked in New York City at NAVA, an architecture and real estate development firm, and Gage/Clemenceau Architects. His past projects in-clude 210 Pacific Street, a passive house de-velopment in Brooklyn, and 14 White Street in Manhattan.

Brendan earned his Master of Architecture from Washington University in St. Louis with a focus on urban housing prototypes in St. Louis, Barcelona, and Seoul, South Korea. He holds an undergraduate degree in Environmental Studies and Studio Art from Dartmouth College.

Education

Washington University, Master of Architecture
Dartmouth College, Bachelor of Environmental Studies

Selected Projects

Troy Boston Amenity Spaces Refresh, Boston, MA—for Equity Residential
One India Street Common Area Improvements, Boston, MA—for Equity Residential
Boutique Hotel at 248 Dorchester Avenue, South Boston, MA—for The Cincotta Companies
Cambridge Street Development, Cambridge, MA— 18 units for Boston Nightlife Ventures
380 Harrison Avenue, Boston, MA— for Related Beal
14 White Street Development, New York, NY (While employed with NAVA in New York)
210 Pacific Street Development, Brooklyn, NY (While employed with NAVA in New York)
80 East 10th Street Development, New York, NY —while at NAVA in New York City, NY

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Khuyen Luong
Architectural Designer

Khuyen joined Utile in September of 2020. He received his Master of Architecture from the Massachusetts College of Art and Design in Boston and Bachelor of Architecture from Ho Chi Minh University of Architecture in Vietnam. With nearly a decade of design experience, he has spent the last seven years as a designer in the United States following two years of commercial and residential work in Vietnam. Khuyen has grown deeply passionate for sustainable design, environmental issues, and resiliency within city development, and has a passion for wood construction and wood architecture. He proudly received an Honorable Mention in the City of Boston's "Living With Water Competition" in 2015.

Prior to joining Utile, Khuyen worked primarily in the higher education sector on numerous large-scale student residences, libraries, and student union buildings projects across North America. He executed a comprehensive design for a mixed-use dormitory in San Francisco, CA, as well as a large-scale passive house dormitory, currently under construction in Victoria, BC, Canada. His involvement included master planning, site development, design, and construction. Outside of his work, Khuyen has participated in numerous architectural competitions internationally, including Waste Stadium, in Nigeria, where he garnered an honorable mention in 2019.

Education

Massachusetts College of Art and Design, Master of Architecture
Ho Chi Minh University of Architecture, Bachelor of Architecture

Selected Projects

1170-1200 Hancock Street Mixed-Use, Quincy, MA—201-unit housing and retail development for FoxRock Properties
25 Sixth Street Housing, Chelsea, MA—62 units of affordable housing for The Neighborhood Developers
Plymouth State University Merrill Place—350 dormitory beds and conference building—while at Perkins+Will in Boston, MA
University of Miami First Year Student Village, Miami, FL—while at Perkins+Will in Boston, MA
University of Victoria Passive House Student Life Project—782 dormitory beds, dining, conference, and academic building—while at Perkins+Will in Boston, MA
MIT Kendall Square Development, MIT Sites 3 & 4—while at Perkins+Will in Boston, MA



Cherry Yang
Architectural Designer

Cherry Yang joined Utile as an architectural designer in September 2020. Cherry aspires to use design to uplift communities and advocate for equitable, diverse futures particularly in residential, cultural, and civic projects. Outside of practice, Cherry enjoys thinking about the curation of historical structures as spectacles for leisurely consumption and the social production of space. She previously worked at Friedrich St. Florian in Providence and Berliner Architects in Los Angeles, where she directed her first design project towards construction for a non-profit afterschool care program.

Hailing from New Zealand with roots in Wuxi, China, Cherry is a BArch '20 graduate from Rhode Island School of Design, where she developed a vested interest in fostering community through presidency for the RISD AIAS chapter. During her time there, Cherry took a semester abroad to ETH Zurich, where she developed a fondness for Swiss construction and non-sliced bread. Cherry has an affinity for climbing rocks, organizing spread-sheets, and cooking without a recipe.

Education

Rhode Island School of Design, Bachelor of Architecture

Selected Projects

- Front Street Development, Portland, ME—113-unit affordable housing re-development for the Portland Housing Authority
- Bowdoin Street Mixed-Use, Boston, MA—41-unit affordable housing and retail development for VietAID
- 152-158 Broadway Passive House, Somerville, MA
- 3371 Washington Street, Jamaica Plain, MA—39-unit affordable senior housing development for Jamaica Plain Neighborhood Development Council and New Atlantic
- Mayfield Junior School Master Plan and Schematic Design, Pasadena, CA—while at Berliner Architects in Los Angeles, CA
- Heart of Los Angeles (HOLA)—while at Berliner Architects in Los Angeles, CA



**Michael Blier, FASLA,
RLA, LEED AP
PRINCIPAL
LANDSCAPE ARCHITECT**

REGISTRATION
Registered Landscape Architect:
MA (#1166), ME (#2600)

EDUCATION

1994
Master of Landscape Architecture, Harvard Graduate School of Design
1986
Bachelor of Landscape Architecture, Bachelor of Fine Arts, Rhode Island School of Design

ACADEMIC EXPERIENCE

2020
Cornell University, Landscape Architecture Department, Visiting Critic
1991-present
Rhode Island School of Design, Landscape Architecture Department, Visiting Critic
1997-2010
Harvard Graduate School of Design, Landscape Architecture Department, Design Critic, Role: Core Studio Instructor and Coordinator, Landscape Representation

AWARDS

2017 BSLA Merit Award	Solitaire Tower, Taichung, Taiwan
2017 Inducted into ASLA Fellows	
2016 BSLA Honor Award	Zelkova Urban Garden, Taichung, Taiwan
BSLA Merit Award	1st and M Apts, Washington D.C.
2012 ASLA Honor Award	200 5th Ave., New York, NY
BSLA Honor Award	200 5th Ave., New York, NY
BSLA Merit Award	Vertical Slice Garden, Taichung, Taiwan
2010 Boston Society of Architects Honorary Membership	
2010 ASLA Honor Award	Theater Group Retreat, Oxford, ME
2009 ASLA Honor Award	Macallen Building, Boston, MA
BSLA Merit Award	Macallen Building, Boston, MA
	Theater Group Retreat, Oxford, ME
	Court Square Press Building, Boston, MA
	Blackstone Power Plant Reno, Cambridge, MA
	Green Roof Award of Excellence,
	Green Roofs for Healthy Cities, Macallen Building, Boston, MA
2006 SLA Excellence Award	Court Square Press Building, Boston, MA
2000 SLA Merit Award: Park Design,	Trampoline and Willow Garden, Chaumont Sur-Loire,
BSLA Merit Award: Park Design	

PROFESSIONAL EXPERIENCE

1996 - present
Founding Principal, Landworks Studio, Inc., Boston, MA
Director of Design for every built and conceptual project, including:
CIVIC
Provincetown Pier; Provincetown, MA
Xiang-Tang Village Master Plan; China
NoMa – MIT Kendall Square; Cambridge, MA
United States Memorial; Wellington, New Zealand
Port Covington Master Plan; Baltimore, MD
Zelkova Public Realm; Taichung, Taiwan
15 Peabody Street Waterfront Park; Salem, MA
Dareen East Waterfront Park; Jubail, Saudi Arabia

INSTITUTIONAL

RISD Quad Study; Providence, RI
West Chester Univ. Landscape Master Plan, West Chester, PA
MIT East Campus/Kendall Sq. Gateway Study; Cambridge, MA
Univ. of New England Student Center; Biddeford, ME
Campus Energy Plant, Tufts Univ.; Somerville, MA
East Boston Public Library; East Boston, MA
Bio-Medical Science Tower 3, Univ. of Pittsburgh; Pittsburgh, PA
Brandeis Science Center Complex, Brandeis Univ.; Waltham, MA
Visitor's Garden, Boston Children's Hospital; Boston, MA
Blackstone Power Station Renovation, Harvard Univ.; Cambridge, MA

RESIDENTIAL/HOSPITALITY

Theater Group Retreat; Western Maine
Macallen Building Condos; Boston, MA
Court Square Press Building; Boston, MA
160 Mass Ave, Berklee College of Music; Boston, MA
Hotel Indigo; Newton, MA
The Park Tower; Taichung, Taiwan
The Solitaire Tower; Taichung, Taiwan
The Garden Tower; Taichung, Taiwan
Station House Apts; Washington, DC
Twenty|20 Condominiums; Cambridge, MA
First + M Apts and 1st Street Park; Wash, DC
Provincetown Inn; Ptown MA, Utile Arch.
One North; Chelsea, MA
LandBay D East; Arlington, VA
MIT Parcel 1 Residential Tower; Cambridge, MA
West of Chestnut Apts; Quincy, MA

WORKPLACE/COMMERCIAL

5600 Columbia Pike; Falls Church, VA
One Broadway; Cambridge, MA
200 Fifth Avenue; New York, NY
Suning IT Office Park & Campus;
Nanjing, China
Nandasoft Corp. HQ; Shanghai, China
eLofts; Alexandria, VA
Sagamore Spirits Distillery; Baltimore, MD



Christopher Macfarlane,
ASLA, RLA
PROJECT DIRECTOR
LANDSCAPE ARCHITECT

REGISTRATION
Registered Landscape Architect:
MA, PA, SC, NH, MD, VA, CT, WI, RI

EDUCATION

1987
Master of Landscape Architecture, University of Pennsylvania

ACADEMIC EXPERIENCE

2020
Cornell University, Landscape Architecture Department, Visiting Critic
1981
Bachelor of Science, Natural Resource Management, Rutgers University

AWARDS

2009 ASLA Honor Award	Macallen Building, Boston, MA
BSLA Merit Award	Macallen Building, Boston, MA
	Theater Group Retreat, Oxford, ME
	Court Square Press Building, Boston, MA
	Blackstone Power Plant Reno, Cambridge, MA
	Green Roof Award of Excellence,
	Green Roofs for Healthy Cities, Macallen
	Building, Boston, MA
2006 ASLA Excellence Award	Court Square Press Building, Boston, MA
2000 ASLA Merit Award: Park Design	
BSLA Merit Award: Park Design	Trampoline and Willow Garden, Chaumont
	Sur-Loire, France

PROFESSIONAL EXPERIENCE

2003 - Present Landworks Studio, Inc., Salem/Boston, MA Director of project manager:

CIVIC

Port Covington Master Plan; Baltimore, MD
Port Covington East Waterfront Park; Baltimore, MD

INSTITUTIONAL

RISD Residential Quad; Providence, RI
West Chester University Landscape Master Plan, West Chester, PA Our Lady of Good Hope Master Plan, WI
Sagamore Spirits Distillery; Baltimore, MD

RESIDENTIAL/HOUSING/HOSPITALITY

Macallen Building Condominiums; Boston, MA
Court Square Press Building, South Boston, MA
Station House Apartments; Washington, DC
Twenty|20 Condominiums; Cambridge, MA
One North; Chelsea, MA
LandBay D East Residential Tower; Arlington, VA
MIT Parcel 1 Residential Tower; Cambridge, MA
West of Chestnut Apartments; Quincy, MA

WORKPLACE ENVIRONMENTS/COMMERCIAL

One Broadway; Cambridge, MA

2000-2003	Construction and Project Management Consultant
1999	Edward Dwight Stone Jr. and Associates, Inc., Fort Lauderdale, FL
	Atlantis Resort and Casino
1998-99	Landworks Studio, Inc., Salem MA (Project Manager)
	Trampoline and Willow Garden, Chaumont Sur-Loire, France
	Theater Group Retreat, Western Maine, USA
1998-97	Joseph A. Wetzel Associates, Boston, MA
1994-96	Martha Schwartz, Inc., Cambridge, MA
1987-95	Coe, Lee, Robinson, Roesch, Inc., Philadelphia PA

Boston Harbor Shipyard and Marina

Boston, MA

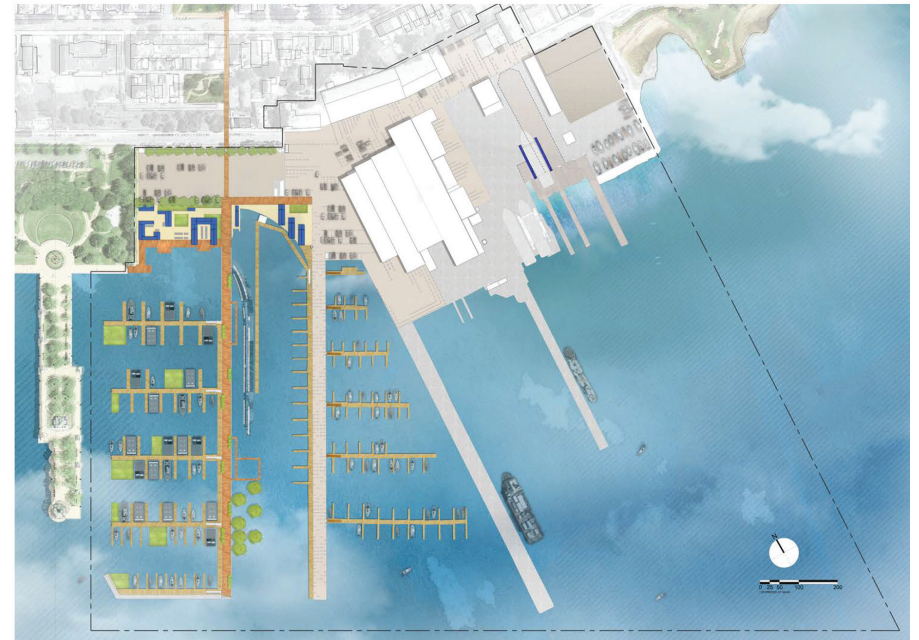
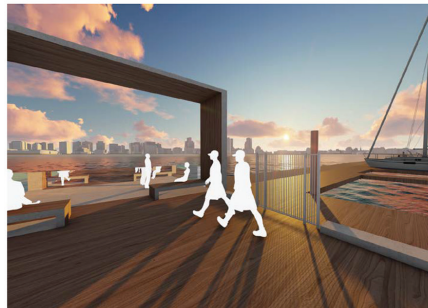
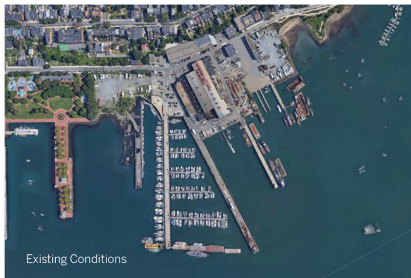
CLIENT: Chuck and Ann Lagasse, Ocean Havens, LLC

PROJECT SIZE: 55 acres

PROGRESS: Master Plan Phase

As one of the last working shipyards in Boston, Boston Harbor Shipyard and Marina is an iconic working landscape; a rich palimpsest of the maritime past, present day mixed uses including a constellation of incubator companies, and public art situated on the Boston Harbor waterfront.

Landworks Studio was brought onto the project to develop a long-term vision that celebrates the existing industrial character while folding in new layers of programming that allow the shipyard to engage the neighborhood and waterfront in new and exciting ways. In the recent past, the client has worked diligently to diversify the occupants of the shipyard, bringing in small local businesses and art institutions to enrich the tapestry of the shipyard. Landworks Studio is building on these efforts to create a distinct landscape that strengthens the connection between the shipyard, its tenants, the East Boston community and beyond. Reimagining the shipyard included: reorganizing and expanding the shipyard's maintenance and storage capacity, reconfiguring the site circulation, integrating new open space, incorporating sustainable practices, expanding access to water, adding a new marina, programming a container city for food and beverage service, and developing a flexible zoning strategy that thoughtfully blends the industrial, commercial, and residential into a seamless landscape that is open and accessible to all.

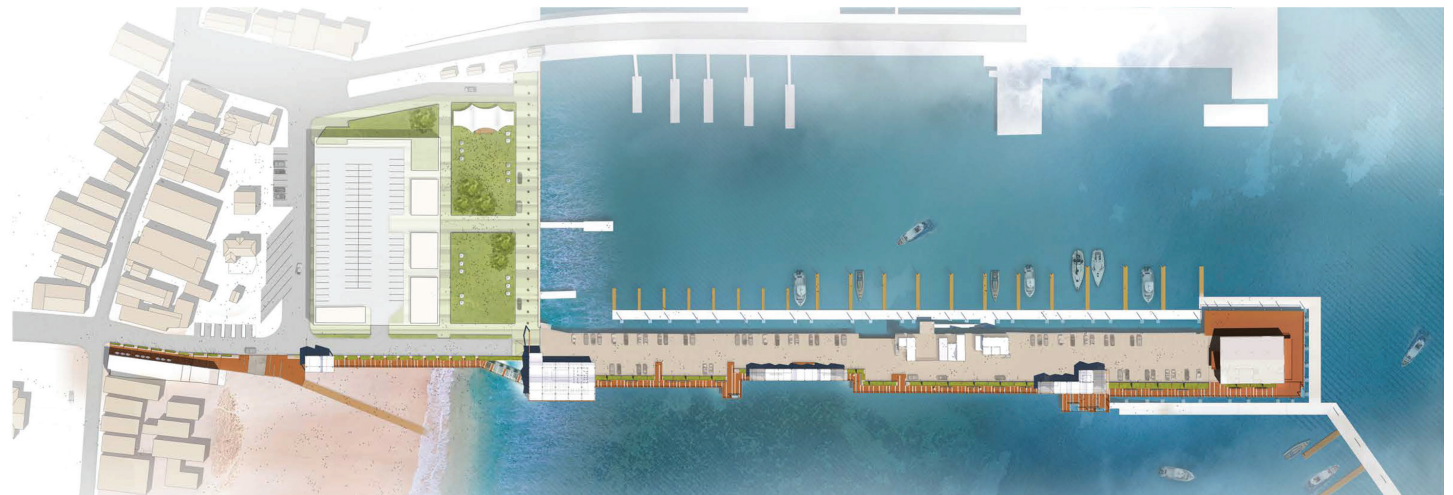
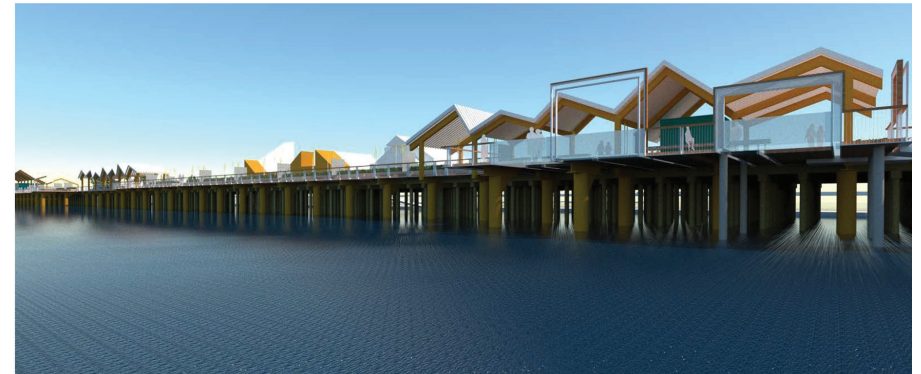
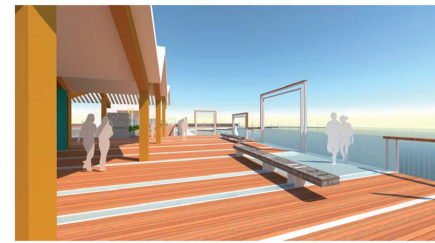
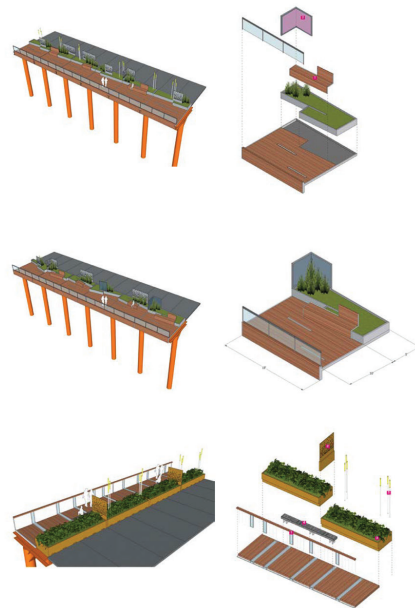


Provincetown Pier and Marina

Provincetown, MA

CLIENT: Chuck and Ann Lagasse, Ocean Havens, LLC
STATUS: Master Planning Phase on-going

An important landmark for Provincetown is being remade. An underutilized 800' long pier is being reimagined to provide public access to the end of the pier and back. New restaurants, comfort stations and shade pavilions and universal access are at the core of the project's objectives. The site and various existing structures are privately owned. However, the client wants to transform this large pier into an active amenity space for both residents and tourists. The design proposes a freestanding linear walkway adjacent to and overlapping the existing pier that will become the basis for other architectural elements as well as a conduit for necessary pier infrastructure, electricity, etc. The architecture of the proposed landscape references both the historic architecture of this coastal fishing community and the language of pier/resort entertainment structures of a by-gone era.



East Waterfront Park - Port Covington

Baltimore, MD

CLIENT: Sagamore Development Company, LLC
 DESIGN TEAM LEAD: Landworks Studio, Inc.
 ECOLOGIST: Biohabitats
 STATUS: Schematic Design completed 2016

The East Waterfront Park represents Phase One of a comprehensive transformation of the Port Covington peninsula in Baltimore. The design of the park provides an active interface between a dynamic urban realm and the rejuvenated water's edge. The language of the landscape emerges from a careful weave of cultural and ecological networks, historic artifacts, and maritime program, which is meant to connect the dots between human and ecological performativity resulting in a more resilient and interactive public realm. A feature of the Park is the reinterpretation of a derelict pier into a linear, interactive ecological experience. New program floats over the existing pier architecture to create innovative new experiences. The Archaeological Pier, as it has become known, will also house the new water shuttle landing with a pedestrian bridge connection over to the reconfigured harbor edge.



East Dareen Beach Neighborhood Park

Jubail, Saudi Arabia

CLIENT: Royal Commission for Jubail
EXECUTIVE LANDSCAPE ARCHITECTS: Landworks Studio, Inc.

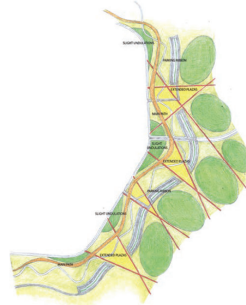
ARCHITECT: CBT+FAEC (Architects/Engineers)
STATUS: Under Construction

East Dareen Beach Neighborhood Park, a 70-hectare park on the Arabian Coast, was the first project in a series of new open spaces in Jubail. It has been designed to serve not only the needs of a rapidly growing city, but also to draw visitors from the greater region. The park is composed of three distinct layers: a linear neighborhood park, a central sculpture park in the form of a series of large earth forms, and an ecological island, which will serve as context for the reestablishment of local ecologies providing significant recreational amenity.

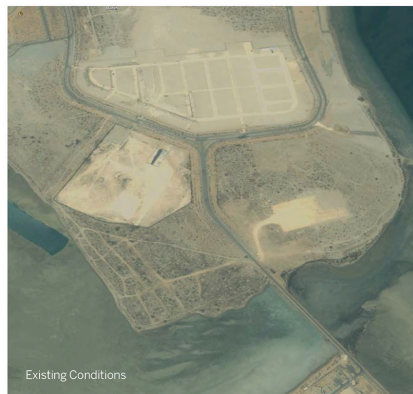
As an expansion of the existing urban fabric and as a waterfront restoration project, we explored several organizational strategies geared towards the establishment of conditions and micro-climates which would bring together cultural activities and ecological systems. Ultimately, we determined that the concept of layering responded to both the growing city and to notions of coastal resiliency and sea level rise.



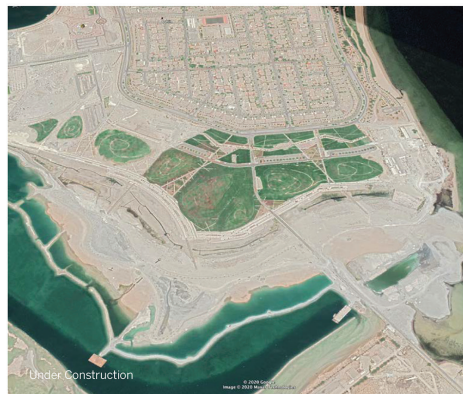
Study of Mound Dimensions



Study of Viewsheds



Existing Conditions



Under Construction





McKenzie & Associates, P.C.
Counselors at Law

183 State Street
Boston, MA 02109
(617) 723-0400
www.mckenzielawpc.com



Contents

I.	Introduction & Firm Overview	2
II.	Firm Qualifications	3
	A. Real Estate & Development	3
	B. Zoning	3
	C. Representative Clients & Transactions	4
	D. Attorney Bios	5
III.	Summary	7

I. Introduction & Firm Overview

McKenzie & Associates, P.C. ("M&A") is pleased to join the development team and provide legal services to Navy Blue LLC ("Navy Blue").

McKenzie & Associates is New England's oldest African-American owned law firm. Since 1984, we have served the legal needs of individuals, families and their businesses; in addition, government agencies, non-profits and private corporations throughout Boston, New England, the United States, and the world. Our practice includes: Business Litigation; Bankruptcy & Creditors Rights; Corporate Counseling, Transactions & Commercial Lending; Estate Planning & Probate; Real Estate & Development; Zoning & Licensing; and representation of Faith-based & Non-profit Organizations.

Conveniently located at 183 State Street in the heart of Boston's Financial District, since 2002 Boston Magazine has recognized the firm as "one of the City's most successful and socially responsible minority-owned law firms." M&A is certified in Massachusetts as a Minority Business Enterprise (MBE). We are guided by the simple but proven philosophy that in making important decisions, there is no substitute for quality, affordable legal advice. By carefully listening to our clients and gaining a thorough understanding of their goals, we successfully guide them through the complexities of statutory, judicial & administrative laws, assist them with their entrepreneurial ventures, help them navigate complex real estate transactions, and serve as general legal counsel on their various other needs.



McKenzie & Associates' State Street offices, in the heart of Boston's Financial District.

II. Firm Qualifications

PRACTICE AREAS



BUSINESS
LITIGATION



BANKRUPTCY
& CREDITORS
RIGHTS



CORPORATE &
TRANSACTIONS



ESTATE
PLANNING
& PROBATE



REAL ESTATE &
DEVELOPMENT



ZONING &
LICENSING



FAITH-BASED
ORGANIZATIONS
& NON-PROFIT

A. Real Estate Development

From adding a roof deck, to converting a family-owned property into condos, to house flipping, to multi-family, commercial, and even hotel developments—we represent some of the most active minority-owned developers of inner-city properties in the Boston area. We advise developers on all aspects of financing, refinancing, land acquisition, negotiating MOUs and other development agreements, sales and leasing, zoning, community processes, construction, and sale to property owners.

We will assist Navy Blue during the development process, including the process of negotiating with the City in the acquiring of City-owned property. We have extensive experience assisting our developer clients acquire parcels from the City's administrative agencies, such as the Real Estate Management and Sales Division of the Department of Neighborhood Development (DND) and the Public Facilities Commission (PFC). Our real estate counsel also frequently works with the City of Boston Corporation Counsel and Tax

Assessor's Office to acquire or redeem properties out of tax foreclosure by the City. We have guided our clients through the City's detailed and complex Request for Proposals (RFP) process, and negotiated tax credit and affordable housing agreements.

Our highly experienced real estate attorneys bring a practical approach to the process of developing real estate, while maintaining a creative approach to problem solving, including project ownership structure & financing, negotiating construction & subcontractor contracts, and solving environmental issues. In addition to access to our extensive City-wide contacts, our clients have the benefit of simultaneous access to our experience in zoning, in order to foster a cohesive development plan, from land acquisition to unit sales/leasing.

B. Zoning

The firm's experience in zoning and licensing matters is unrivaled particularly in the City of Boston and surrounding communities. We have provided representation to clients before both state and local administrative bodies such as the Zoning Board of Appeal, Boston Planning & Development Agency (BPDA), Inspectional Services Department (ISD), the Licensing Board, the Alcoholic Beverages Control Commission, the Cannabis Control Commission, and the Consumer Affairs Division. We also have extensive experience with the zoning of City-owned parcels, and have guided our clients through development of



REAL ESTATE &
DEVELOPMENT

McKenzie & Associates represents some of the most active and reputable minority-owned developers of inner-city properties in the Boston area on all aspects of financing, refinancing, land acquisition, sales and leasing, zoning, community processes, construction, and sale to property owners.

II. Firm Qualifications (cont'd.)

parcels acquired from the Department of Neighborhood Development from the RFP stage through occupancy.

Attorney Joseph D. Feaster, Jr., who leads our Zoning & Licensing practice, has over 20 years of zoning and licensing practice experience, and formerly served on the Boston Board of Appeal for 11 years, 3 of which he served as Chair.

Throughout his many years of practice, he has developed an unparalleled understanding of the zoning process in Boston, as well as invaluable contacts in the various state, local, and City of Boston government and agencies. From a simple addition of a roof deck to the construction of a hotel, Attorney Feaster has successfully represented a wide variety of development interests throughout the City of Boston. This experience provides an invaluable advantage to Navy Blue in getting its project(s) through the City of Boston zoning approval process.



Our firm has significant experience in zoning and licensing, representing clients before both state and local administrative bodies such as the Zoning Board of Appeal, Boston Planning & Development Agency (BPDA), Inspectional Services Department (ISD), the Licensing Board, the Alcoholic Beverages Control Commission, the Cannabis Control Commission, and the Consumer Affairs Division.

C. Representative Clients & Transactions

Below are some of the real estate development and zoning projects on which we have represented developers large and small, and of all experience levels throughout the years:

- **Boston Redevelopment Authority Parcel P3.** We guided our client through the zoning and community processes for a multi-use development project on Boston Redevelopment Authority Parcel P3 for retail, hotel, office, daycare, entertainment space, housing, and parking.
- **Boston Redevelopment Authority Parcel 9.** We represented the developer of a mixed-use development comprised of a 145 room hotel, 50 residential units, ground floor retail/commercial space, and on-site accessory parking for 122 vehicles on Boston Redevelopment Authority Parcel 9, Boston MA.
- **Cruz Development Corporation.** For over 20 years we have served the Cruz Companies in the acquisition, development, and zoning & permitting matters. Most recently, we handled zoning and other legal matters for the construction of an approximately 82,800 ft.², 55 Unit, residential project with two levels of commercial space, and two levels of parking in Dudley Square, Boston (Roxbury) MA. We are currently representing Cruz on the proposed construction of a 150 Unit, mixed-income housing project, also in Roxbury.
- **Cummins Development LLC.** We represented Cummins Development LLC before the Boston Redevelopment Authority in their efforts to construct scattered site housing on several parcels in Boston.
- **Harbor Health Services, Inc.** We represented Harbor Health Services, Inc. in the Boston Redevelopment Authority's Article 80 development review process and before the Board of Appeal and Inspectional Services Department in connection with the construction of an Adult Day Center and offices.
- **JP Parcel U, LLC.** We represented the developer in a \$40 million project on a mixed-use development consisting of 48 condominium units and 76 affordable housing units, community space, and parking for 90 vehicles.
- **Long Bay Commercial Properties.** We represent the developer of a proposed 25-story, 241 Unit residential project in Dudley Square, Boston (Roxbury) MA.

(cont'd.)

II. Firm Qualifications (cont'd.)

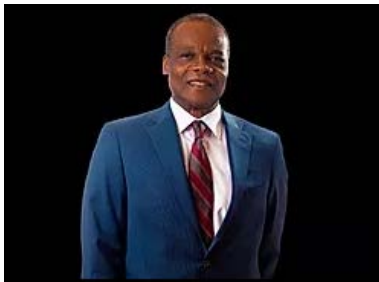
C. Representative Clients & Transactions (cont'd.)

- **Madison Park Development Corporation.** We represent the developer of a proposed multi-family residential development with 20 Units of affordable housing and ground floor commercial space in Dudley Square, Boston (Roxbury) MA. We also represent this developer in a Project Change for its proposed Parcel 10 project, which was originally proposed for commercial (retail/office) use, but is now proposed as a 6-story 144-unit mixed-use residential development.
- **Melbourne Street Partners, LLP.** Years ago we began representing three young real estate entrepreneurs in the acquisition of City-owned and distressed properties. We continue to represent the partnership and its now-multimillion dollar portfolio in development, zoning & permitting matters, condominium conversions, and conveyancing to buyers.
- **Vaughan Avenue Group LLC.** We represent an experienced civil engineer and first-time developer of a proposed multi-family residential development with 55 Units and 20 garage spaces in Boston (Dorchester) MA.

D. Attorney Bios

The attorneys primarily responsible for this engagement are Denzil D McKenzie, Joseph D. Feaster, Jr., Timothy A.M. Fraser and Deonee S. High.*

*admission pending in January 2021.



DENZIL D. MCKENZIE

Founder & Managing Director

Email: dmckenzie@mckenzielawpc.com

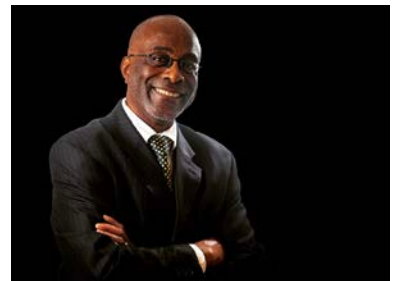
Phone: (617) 778-0010

Attorney Denzil D. McKenzie is the managing director and founder of McKenzie and Associates, P.C. and leads the firm's Commercial Finance & Corporate Transactions practice. He is a graduate of Boston University School of Law and studied tax law in the Boston University Graduate Tax Program. Attorney McKenzie began his legal career as an Assistant Attorney General for the Commonwealth of Massachusetts. Attorney McKenzie is well recognized among practitioners involved in representing family-owned business and non-profit organizations throughout the Commonwealth of Massachusetts. He often serves as outside counsel to small and mid-sized corporations and non-profit organizations, including health and human service providers and churches. He has over 40 years of experience practicing law in the areas of business litigation, probate administration, bankruptcy, tax appeals, and business law.

Attorney Joseph D. Feaster, Jr. has been practicing law for nearly 45 years, during which time he has developed an expertise in numerous areas of the law, including corporate, employment and labor, real estate, contracts, licensing and zoning, and probate. Presently, he is the court-appointed Receiver for Roxbury Comprehensive Community Health Center.

Attorney Feaster previously served as Interim Town Manager of the Town of Stoughton, as a member and Chairman of the City of Boston's Board of Appeal, as President of the Massachusetts Community and Banking Council, as Acting Director of Real Estate for the Massachusetts Turnpike Authority, as Interim Administrator of the Boston Housing Authority, one of the largest public housing authorities in the country; as Assistant

(cont'd.)



JOSEPH D. FEASTER, JR.

Of Counsel

Email: jfeaster@mckenzielawpc.com

Phone: (617) 778-0012

D. Attorney Bios (cont'd.)

Secretary and General Counsel in the Commonwealth's Executive Office of Administration and Finance, as Associate Counsel in Prudential Insurance Company's Northeast Home Office, and as an attorney at the National Labor Relations Board's Boston Regional Office. His professional affiliations are numerous, as his expertise is sought within the City of Boston and nationally. Attorney Feaster previously served as the Senior Vice President of Victory Group, a government and community relations firm, and is also President of Feaster Enterprises, a strategic planning, organizational development, and community outreach consulting firm. He has served as an adjunct professor in Northeastern University's Master in Public Administration program, and as a research associate at the William Monroe Trotter Institute at the University of Massachusetts at Boston.

Attorney Feaster received his Juris Doctor from Northeastern University School of Law. He has also completed programs at the Massachusetts Institute of Technology's Center for Real Estate Development and Harvard University's John F. Kennedy School of Government. Attorney Feaster is admitted to practice before the courts in Massachusetts, the U.S. District Court of Massachusetts, the First U.S. Circuit Court of Appeal, and the U.S. Supreme Court.

Attorney Timothy Fraser is an Associate Attorney with over 15 years of experience as a transactional attorney. He is a graduate of Hampton University in Hampton, VA and received his Juris Doctor in Taxation from Suffolk University Law School, attending the evening program while working full-time as a legal assistant at McKenzie & Associates. Since graduating Suffolk Law and being admitted to the Massachusetts Bar, Attorney Fraser has specialized in corporate mergers & acquisitions, most recently with the tax practice at global consulting firm

PricewaterhouseCoopers LLP. Prior to PwC, he worked in the M&A practice at Deloitte & Touche LLP. During this time, Attorney Fraser concentrated his practice on the purchase, sale, and other transactions involving companies in the food, beverage, apparel, and lifestyle industries. Attorney Fraser returned to McKenzie & Associates in 2019. His practice includes corporate transactions, estate planning & administration, and real estate & zoning.



TIMOTHY A.M. FRASER

Associate Attorney

Email: tfraser@mckenzielawpc.com

Phone: (617) 778-0015



DEONEE S. HIGH

Associate Attorney

Email: dshigh@mckenzielawpc.com

Phone: (617) 778-0013

**admission pending in January 2021.*

Deonee S. High is an Associate Attorney with McKenzie & Associates, P.C. Attorney High brings a unique perspective based on her background in social work and practices in the areas of Civil Litigation, Business Law, and Probate.

Attorney High earned her Bachelor of Arts in Criminal Justice with Special Honors and a Minor in Psychology from George Washington University in Washington, DC before earning her Juris Doctor from Michigan State University College of Law in East Lansing, Michigan. While in law school, Attorney High was actively involved with the Public Interest Law Society, the Trial Advocacy Board, and the Black Law Students Association. Attorney High developed litigation skills and honed her passion for client advocacy while competing in the American Bar Association's Arbitration Competition, where she served as a team captain and brief writer,

helping MSU Law become Regional and National Finalists for two consecutive seasons. In January 2021, Attorney High will be admitted to practice law before the Massachusetts Bar.

II. Firm Qualifications (cont'd.)

Our attorneys collaborate as a team and leverage our combined talents to provide solutions for our clients. Our ability to provide successful outcomes has enabled us to develop long-term relationships with our clients, often serving as trusted advisors to them on both business and personal matters.



Denzil D. McKenzie
*Founder & Managing
Director*



Linda M. Davidson
Of Counsel



Joseph D. Feaster, Jr.
Of Counsel



Timothy A.M. Fraser
Of Counsel



Deonee S. High
*Associate Attorney**



Andrea M.A. Osborne
Of Counsel

III. Summary

During the course of over 35 years in business, McKenzie & Associates, P.C. has represented for-profit and non-profit developers large and small, and of all experience levels, advising them on all aspects of residential and commercial real estate transactions: financing; commercial & residential property and land acquisitions; development; zoning & community processes; permitting; construction; refinancing; and sales & leasing.

Attorney Denzil D. McKenzie is the managing director and founder of McKenzie and Associates, P.C. and leads the firm's Commercial Finance & Corporate Transactions practice, advising on real estate, commercial lending, and other transactional matters. Joseph D. Feaster, Jr., who leads our Zoning and Licensing practice, formerly served on the Boston Board of Appeal for 11 years, 3 of which he served as Chair. He has over 20 years of zoning and licensing practice experience and is widely regarded as an expert on zoning and affordable housing matters. Timothy A.M. Fraser has worked with Attorney Feaster for years on zoning, permitting, and real estate development matters. Our remaining team of attorneys offers a collective 150 years of experience across a diversity of legal disciplines. We collaborate as a team and leverage the combined talents of all our attorneys to provide solutions for our clients, and our ability to provide successful outcomes has enabled us to develop long-term relationships with our clients.

We thank you for taking the time to discuss the Navy Blue development project with us, as well as for the opportunity to join the Navy Blue development team on this innovative and intriguing project.

Warmest regards,

McKenzie & Associates. P.C.

DENZIL D. MCKENZIE
Founder & Managing Director
dmckenzie@mckenzielawpc.com
o. (617) 778-0010
c. (617) 510-5212

JOSEPH D. FEASTER, JR.
Of Counsel
jfeaster@mckenzielawpc.com
o. (617) 778-0012



About Our Firm

Located in Government Center in downtown Boston, Fort Point Associates, Inc. (FPA) is a multi-disciplinary professional firm that provides urban planning, environmental consulting and permitting, and project management services to public and private sector clients. Over the past 35 years, FPA has established itself as a leader in managing urban and waterfront master plans, institutional expansions, private real estate developments, and complex public infrastructure improvements. FPA enjoys an enviable reputation with its clients for its professionalism and adeptness in dealing with the many federal, state, and local agencies relevant to planning and development.

FPA's team of highly experienced professionals counsels clients on approaches to planning and development projects, providing advice on effective strategies for facilitating approvals, and for managing the public review and community participation processes. With its breadth of professional expertise from fields including planning, architecture, historic preservation, landscape architecture, transportation, real estate development, and law, FPA offers insights and guidance that result in time and cost savings. Senior professionals dedicate their personal attention to every project to ensure effective relationships with interest groups including public agencies, private property owners, advocacy organizations, neighborhood associations, and others.

With its involvement in such high-profile undertakings as the \$14 billion Central Artery/Tunnel Project, the Boston Convention and Exhibition Center, and the \$2.5 billion Encore Boston Harbor Resort, FPA has longstanding relationships with key government personnel and closely monitors trends and changes to existing statutes and regulations. Its single office is located only minutes from state and city government offices in Boston, allowing it to keep abreast of the individual preferences and informal policies that operate alongside the laws and regulations that govern agencies' day-to-day activities. Although many of its projects are focused in the Boston area, FPA has successfully completed hundreds of projects throughout the Commonwealth of Massachusetts.

In 2018, FPA was acquired by Tetra Tech, an international leader in consulting, engineering, and technical services. With 20,000 employees located in 450 offices worldwide, Tetra Tech's technical expertise greatly complements FPA's own and bolsters its ability to offer comprehensive solutions to clients' needs. Although acquired in full, FPA continues to operate as Fort Point Associates, Inc., a Tetra Tech Company, a stand-alone entity within the Tetra Tech organization.

Urban and Waterfront Planning

FPA has led a broad range of urban and waterfront master plans for public agencies including the Boston Planning & Development Agency, Massachusetts Port Authority, and MassDevelopment. FPA has earned a reputation as a leader in Harbor Planning due to our in-depth understanding of the emerging issues of climate change and resiliency and the Massachusetts Chapter 91 Regulations and the mechanisms through which cities and towns can tailor them to their unique needs. Balancing commercial, residential, and public interests to create vibrant and prosperous waterfronts, we work collaboratively with all agencies and interested parties to generate strong, long-term, and realizable visions for the future.

Environmental Regulatory Consulting

FPA brings its extensive experience and informed analysis to bear on the resolution of complex environmental issues. With a highly sophisticated grasp of municipal, state, and federal environmental regulations, we provide strategic advice regarding options available to our clients to comply with regulations while achieving desired outcomes. We have developed excellent working relationships with relevant environmental agencies, including the Massachusetts Executive office of Energy and Environmental Affairs, the Department of Environmental Protection, and the office of Coastal Zone Management. Equally, we take a collaborative approach to working with relevant environmental advocacy groups, and have a proven track record of consensus building.

Community Relations

FPA excels at artfully managing the dynamic relationships between diverse stakeholders to work toward mutually beneficial outcomes. FPA assists its clients by productively channeling the public's involvement in the development process through pro-active community outreach programs and effective communication with the various parties of interest to a project. FPA has demonstrated its commitment to Greater Boston and has developed strong and positive relationships with leaders of community groups. We are skilled in diffusing escalating conflicts and building trust and a spirit of cooperation amongst parties while continuing to move our clients' projects forward.

Project Management

FPA boasts exceptional project management expertise that ranges from guiding the development of individual properties to conducting visioning for large-scale, urban areas. FPA has established its reputation managing complex, multi-dimensional projects, including as an on-call consultant to Boston University for construction projects totaling over \$500 million over the span of 30 years. Because of the breadth of its experience and the diverse capabilities of its staff, FPA frequently assumes the role of lead consultant. We are highly capable of orchestrating the services of a multi-disciplinary team to achieve project objectives on time and on budget. As project manager, FPA regards itself as responsible for ensuring that the team's professional resources are applied appropriately, fully capitalizing on the strengths and expertise of each contributing team member.

Staff Disciplines

FPA's team of nine (9) highly experienced professionals range in disciplines from environmental law and policy to landscape architecture and urban design. Building on a shared skillset of project sensitivity and thoughtful conflict resolution, each team member offers their own expertise in specialty areas including urban and environmental planning, landscape architecture, maritime affairs, environmental law, sustainability design, and climate change analysis and planning. FPA is known for being a highly skilled firm capable of counseling clients on approaches to planning and development, offering effective strategies for facilitating approvals, and managing the public review and community participation processes.



Jamie M. Fay, AICP, CEP

President

Summary

Mr. Fay is the founder and president of Fort Point Associates, Inc., a multi-disciplinary planning and environmental consulting firm. Mr. Fay has been the principal-in-charge and lead consultant for the past 35 years for a variety of master planning, real estate development and public infrastructure projects. Notable public sector projects include the \$2.5 billion Encore Boston Harbor Resort, the \$850 million Boston Convention and Exhibition Center, and the \$14 billion Central Artery/Tunnel project. Private sector projects include over \$4 billion in real estate development for retail, commercial, industrial, residential and institutional uses. Mr. Fay is a member of the American Institute of Certified Planners and a Certified Environmental Planner, the former Chairman and member for 30 years of the Ipswich Finance Committee, Vice President and Trustee of The Boston Harbor Association in Boston for 25 years, Trustee of Boston Harbor Now, and a member of the Public Affairs committee of the National Association of Office and Industrial Properties.

Relevant Experience

Encore Boston Harbor Resort and Casino – Everett, MA

Mr. Fay was the principal environmental consultant for the competitively bid and awarded Region 1 Gaming License for the new \$2.5 billion Wynn Boston Harbor Resort and Casino in Everett, MA. Mr. Fay provided strategic advice on the environmental and land use approvals strategy for the project, including extensive and detailed review of the controversial project through the state MEPA process, development of a “living shoreline” as part of the project site, climate change and resiliency strategy for the facility, preparation and approval of the Everett Central Waterfront Municipal Harbor Plan, local, state and federal permits, all on an accelerated schedule. A total of five environmental legal appeals were filed on this controversial project and all were denied by the courts due to the diligent attention to detail and comprehensive documentation included in filings.

Environmental Assessments, Statewide

Over the past 27 years as president of Fort Point Associates, Mr. Fay has been the primary author of well over 200 Environmental Notification Forms (ENFs), Environmental Impact Reports (EIRs), Environmental Impact Statements (EISs), Project Impact Reports (PIRs) and Environmental Reevaluations (ERs) at the local, state and federal levels. Project types included transportation infrastructure (highway, rail, port), residential (single family, condominium, golf course community), office (high rise, low rise), commercial (box retailers, shopping center, specialty retail), institutional

(hospital, university) and industrial (manufacturing, distribution). In addition to the \$14 billion Central Artery/ Tunnel project, combined value of private projects reviewed under MEPA exceeds \$5 billion.

Boston Convention and Exhibition Center – Boston, MA

Mr. Fay was the local project manager for the interim and final planning/feasibility study for the new \$850 million Convention Center in South Boston. The study was prepared for a joint working group of the Senate, House of Representatives, City of Boston and Executive Office of Administration and Finance. The \$700,000 study was completed in five months and addressed all of the economic, community, programmatic, environmental and operational issues surrounding the facility. On the basis of the report, legislation was filed by the Governor and approved by the legislature. Mr. Fay subsequently prepared the Project Development Plan for approval by the Boston City Council, and the Expanded Environmental Notification Form, Final EIR for the Boston Redevelopment Authority and obtained all project permits and approvals.

Central Artery/Tunnel – Boston, MA

As a senior environmental strategist for joint venture providing environmental approvals for all activities proposed in conjunction with the Central Artery/Tunnel project for the Massachusetts Highway Department (MHD), Mr. Fay was responsible for providing the MHD with an environmental approvals acquisition strategy; providing Quality Assurance/Quality Control on permit submissions to agencies; recommending strategic approaches to the resolution of environmental controversies; and providing day-to- day liaison with environmental agencies. At \$14 billion, this project was the largest public works projects in the nation and required the acquisition of over seven hundred federal, state and local environmental approval actions covering work in some 150 separate construction contracts throughout Boston and Cambridge.

Education

B.A., 1976: Hampshire College, Amherst, Massachusetts

Professional Affiliations

- Member, American Institute of Certified Planners
- Trustee, Boston Harbor Now
- Former Trustee and Executive Committee Member, Boston Harbor Associates (25 years)
- Finance Committee Member, Town of Ipswich (30 years)
- Public Affairs Committee Member, NAIOP Massachusetts Chapter
- MassDEP Regulatory Reform Task Force

Publications and Presentations

“Waterfront Resiliency: Architecture and Site Strategies,” AIA National Convention, June 2019

“Coastal Development Best Practices” Urban Land Institute Coastal Forum, October 2018

“Rising Sea Levels; Design Strategies for Waterfront Projects,” ABX, November, 2016

“Industrial Development and Clean-up of Mystic River,” Mystic River Watershed Association/Tufts Institute on the Environment, October 28, 2014

“Climate Resilient Housing Types,” Architecture Boston Expo, October, 2014



Charlestown Marina

Charlestown, Massachusetts

Charlestown Marina, LLC, the owners of the Charlestown Marina, retained Fort Point Associates, Inc. to provide municipal, state, and federal permitting services for the reconstruction of an existing marina.

The owners purchased the marina, which includes facilities at both Pier 6 and Pier 8, in a dilapidated state and under enforcement proceedings. Fort Point Associates provided regulatory consulting and collected documentation and submittals related to the demolition and removal of previous marina structures as required by the Superior Court's Consent Judgment. At Pier 8 the marina has been reconstructed into a state-of-the-art facility with full accessibility at 207 dedicated slips and room to accommodate a total of 260 vessels. The Pier 6 facility design is being reassessed for sustainable improvements to the restaurant building.

FPA secured approvals from the Massachusetts Executive Office of Energy and Environmental Affairs, the US Army Corps of Engineers, the Massachusetts Department of Environmental Protection, the Department of Marine Fisheries, and the Boston Conservation Commission.

Client

Charlestown Marina LLC

Client Contact

Charles A. Lagasse

Services

Environmental Consulting,
Project Permitting

Status

Pier 8 - Complete

Pier 6 - In Progress

Construction Cost

Pier 8 - \$15 Million

Pier 6 - Pending



Liberty Wharf

South Boston, Massachusetts

FPA was retained by Cresset Development LLC to manage the environmental review process for the award-winning Liberty Wharf Development.

Completed in early 2011, the project is a \$60 million, mixed-use complex comprised of three buildings constructed on a rebuilt pier in Boston's emerging Seaport District. The former site of Jimmy's Harborside Restaurant, it is now home to five restaurants, an outdoor public waterfront plaza, 28,000 square feet of office space, and a public marina.

FPA managed the complex public approval and permitting process, which involved working within a multifaceted interagency memorandum and engaging adjacent commercial fishing interests as well as other local interests to ensure project support. Since completion, the project has had a transformative impact on this burgeoning district, spurring activity and vibrancy to the area.

Client

Cresset Development LLC

Client Contact

Ed Nardi, President

Services

Environmental Permitting,
Public Approval Process

Status

Complete

Construction Cost

\$60 million



ONEWAY
Build Your Way

Federal Capabilities Statement



One Way Development, Inc.
67 Kemble St Suite 1.1, Boston, MA 02119
Office: 617.390.5469 | Fax: 617.390.5472 | www.onewaydevelopment.com
8(a), SDO, M/DBE, Sec 3, and DCAMM

Contents

<i>Our Vision and Objectives</i>	3
1.0 Relevant Experience.....	3
2.0 Company Profile.....	5
2.1 One Way Development Profile	5
2.2 One Way Development SBA Profile	6
2.2.1 Safety: Our top Priority	6
2.2.2 Certifications.....	6
2.2.3 Key Personnel	7

Our Vision and Objectives

One Way Development is a general construction company where our employees and management are committed to a quality construction experience by creating a safe, team oriented and efficiently run project. We guarantee that our customers will be excited with the final result so that our legacy of construction excellence will continue well into the future.

Located in Boston, Massachusetts, One Way Development, Inc. ("OWD") is a general construction contractor catering to the commercial, residential and government sectors throughout Greater Boston and New England. Our clients include a wide array of, business real estate developers, and government agencies. One Way Development has developed a stellar reputation for providing our clients with fine-quality and timely work at a surprisingly reasonable price. We're your one-stop full service general construction contractor.

One Way Development, is a minority-owned and operated company, which has been in business for more than 15 years. Founded by Abraham Gonzalez, OWD's team consists of a team of professional and highly skilled construction staff, estimators, project managers and accounting personnel.

OWD entered the Small Business Administration's 8(a) Program and has expanded its clientele to include various Federal Government agencies, including the General Services Administration, the Navy, US Fish and Wildlife, Bureau of Prisons, the National Park Service, the US Coast Guard and FEMA. In 2018 Mr. Gonzalez added an Electrical division to OWD's inhouse construction staff to further service our clients' needs. Mr. Gonzalez has carefully expanded the business each year to increase gross revenues, enlarge the geographic reach of the company, and increase the level of in-house services offered by the firm.

1.0 Relevant Experience

Project Title	Amount	Completion
USDA Lab, Bath Camp Edward	\$ 265,743.49	9/2/15
USCG Deer Island Light	\$ 482,500.00	10/9/15
FCI Danbury Intrusion CT	\$ 237,262.00	2/22/18
USCG Jonesport WaterMain ME	\$ 17,141.00	5/26/15
USCG Fuel Tanks Maine	\$ 283,872.00	11/18/16
USCG Sewer Repair I, MA	\$ 51,803.00	8/7/15
USCG Martha's Vineyard	\$ 2,521,175.78	4/20/16
USCG Sewer II, Cape Cod MA	\$ 109,369.00	10/14/15
NPS Longfellow	\$ 19,400.00	11/6/15
AFB Hanscom Steam	\$ 14,993.00	5/2/16
USCG Point Judith	\$ 168,617.00	5/26/16
FEMA Logistics Room Drain	\$ 9,705.00	10/24/15
FEMA Handicap Ramp Concrete	\$ 2,660.00	10/24/15
FEMA Exterior Stair Tread	\$ 9,800.00	10/24/15
USCG MV Windows	\$ 152,258.00	8/15/16
Navy Newport, RI Al Stand	\$ 97,163.00	7/31/16



Hog Island Light, MA	\$ 139,962.00	7/19/17
FEMA Painting, MA	\$ 11,201.00	6/4/16
Wynn Casino	\$ 1,379,027.00	4/7/17
Navy Pking Lights, NY Sch/Ro	\$ 274,627.00	8/17/16
Navy IDIQ-Fall Protect	\$ 233,439.00	11/14/16
Navy IDIQ Bridge 2 Lighting	\$ 65,409.00	12/15/16
NPS Charlestown Scale House	\$ 371,163.00	7/31/17
FEMA Furniture	\$ 44,406.66	9/16/16
USCG MV Waterline	\$ 141,826.00	11/12/16
Navy Bldg 7, Kittery, ME	\$ 86,477.00	11/27/16
Navy Buoys Kittery, ME	\$ 34,082.00	12/18/16
FCI Danbury, Front Entrance	\$ 39,824.00	10/19/16
Navy IDIQ Elect Distribution	\$ 198,098.00	12/30/16
FCI Danbury Power Plant	\$ 149,854.00	12/4/16
Navy IDIQ Bldg 92 Low Voltage	\$ 95,680.00	1/23/17
NPS Charlestown Bldg 265	\$ 924,217.00	7/19/17
Hanscom AFB Steam 2	\$ 24,965.00	9/30/16
FEMA Soundproofing	\$ 113,844.00	5/5/17
FEMA Shelter	\$ 54,997.00	10/31/16
NPS Floyd House	\$ 399,966.00	5/31/17
USCG Hull HVAC Repair	\$ 62,775.00	1/13/17
MV Waterline Small	\$ 242,112.00	2/2/18
Navy CNY Lintels	\$ 42,412.00	3/24/17
Navy IDIQ Bldg 24 Stairs	\$ 242,112.00	2/2/18
Navy Bldg 24 Bathrooms	\$ 364,633.00	12/17/17
Brooke Charter School	\$ 444,000.00	4/4/18
FCI Danbury Plumbing	\$ 15,295.00	9/23/17
USCG Menemsha Louvers	\$ 14,632.00	9/30/17
FEMA Furniture 3	\$ 15,999.00	11/24/17
FEMA UPS	\$ 380,545.00	3/29/18
Navy Buffalo, NY Carpet	\$ 55,107.00	4/25/18
Navy Buffalo Drill Deck	\$ 92,535.00	9/4/18
TSA Logan Airport Electrical	\$ 104,828.00	12/21/17
USCG Rockland Bird Block	\$ 1,759,355.43	5/31/19
USCG Beverly Windows	\$ 89,715.00	9/13/18
USCG Academy Exterior Doors	\$ 63,733.00	9/17/18
Navy Buffalo Door Hardware	\$ 49,131.00	10/29/18
USCG Provincetown Waterline	\$ 218,121.00	1/4/19
USFW Sudbury Storage Building	\$ 698,790.35	8/31/19
USCG Staten Island Dispenser	\$ 489,327.00	1/18/19
USCG Hampton Bays HVAC	\$ 464,399.00	12/27/18
USCG Cape Cod Sewer Line	\$ 46,979.00	10/13/18
USCG Manhattan Brow Pier	\$ 432,570.00	5/17/19

USCG Cape Cod Flooring	\$ 115,767.00	1/7/19
USCG Hull Generator	\$ 24,499.00	4/2/19
USCG Provincetown Windows	\$ 365,930.00	6/26/19
USCG New Castle	\$ 175,754.00	5/17/19
USCG Hampton Bays Emerg Repair	\$ 150,395.00	3/22/19
Navy White River Junction	\$ 55,431.00	8/9/18
FEMA Cooling Tower VFD	\$ 28,817.00	6/10/19
USCG Boston Condenser Water	\$ 485,881.00	3/13/20
FEMA Ductwork	\$ 22,735.00	6/28/19
FEMA Region 1 RRCC Updates	\$ 106,165.00	8/22/19
Suffolk UPS Bldg Logan	\$ 2,315,549.00	2/28/20
Navy - PNSY 344 and 345 Rigger	\$ 387,499.00	6/2/20
USFW Oxbow NWR Walkway System	\$ 190,155.00	12/31/19
Navy PSNY Replace Tank	\$ 225,682.24	4/27/20
USCG Deck Repair	\$ 258,874.12	8/12/20
USCG Replace Exterior Doors	\$ 264,174.00	
Navy - Quincy NOSC Controls	\$ 722,592.00	10/31/20
Navy PNSY Replace Chiller	\$ 635,083.65	2/28/20
Navy CNY Building 5 Locks	\$ 58,505.00	10/22/20
USCG Scissor Dock	\$ 10,989.00	9/24/20
FEMA Roof	\$ 188,384.20	10/13/20
FEMA Carpet	\$ 146,601.00	12/9/20
Navy PNSY Fire Alarm B300/170	\$ 689,479.00	
USFW N Attleboro Pools Rehab	\$ 52,202.00	

2.0 Company Profile

2.1 One Way Development Profile

Company Profile, to include number of offices and number of employees per office, annual revenue, office locations, available bonding capacity, DUNS number, and CAGE code

One Way Development DUNS: 793526463 One Way Development CAGE: 63GL5

One Way Development currently employs over 30 talented Craftspeople Mechanics and Supervisors from our office in Boston, MA. We hold Massachusetts Construction Supervisor and Home Improvement Contractor licenses. We also hold numerous certifications in the fields of solar installation, lead safe repair painting, lead abatement, residential conservation services, weatherization, and air sealing and insulation.

In 2020 we expect revenues to be approximately \$15M, with substantial growth in our depth of experience. Our key personnel, selected precisely for their ability to responsibly manage our construction projects, have over a century of combined Construction experience, much of that



with the Federal Government. Our Single Project Bonding Limit is currently \$1.5MM-10.5MM Depending on current backlog.

2.2 One Way Development SBA Profile

Please indicate If you are a Small Business, Small Disadvantaged Business, Woman Owned Small Business, Service-Disabled Veteran-Owned Small Business, certified HUBZone Small Business, and certified 8(a) Businesses

One Way Development is certified by the Massachusetts State Office of Minority and Women Business Assistance as a Minority Business Enterprise/Disadvantaged Business Enterprise (MBE/DBE). OWD was also recently certified by the United States Small Business Administration under the Federal 8(a) contracting program for growing minority businesses.

Our initial approach to working in the 8(a) program has been to consistently increase the number and size of Government projects through the competitive bidding process so that OWD has the necessary skills and competitive edge in completing Federal projects. As a result of the projects that we have completed, we have a solid resume of Federal/Institutional tasks under our belt with a solid knowledge of government processes and procedures. In addition, so that we are able to provide the highest quality personnel available to work on projects, we have been actively training our staff in Quality Control and Safety Management taught by the high standards of the Navy and Army Corps of Engineers.

2.2.1 Safety: Our top Priority

Like all successful companies, we place employee safety as a top priority. Our goal is zero accidents and injuries. We have had remarkable results thus far with our safety program, lowering our experience modification rates each year. We develop a project specific safety program for each contract. Our program contains emergency information, weekly meeting forms, and accident reporting forms. We also provide a thorough activity hazard analysis (AHA) of each work activity involved with the project. We outline the potential dangers of performing that activity, methods to prevent injury and the protective equipment to be used. Finally, our safety plan provides a comprehensive description of our safety requirements and procedures.

2.2.2 Certifications

One Way Development holds Massachusetts Construction Supervisor and Home Improvement Contractor licenses. We are fully trained and qualified in Solar Photovoltaic installation and certified in Massachusetts with the following distinctions:

- Certified in the 8(a) contracting program by the US Small Business Administration
- Lead Safe Renovator Repair Painting contractor licensed by the Federal Environmental Protection Agency (EPA)
- Lead Abatement Contractor licensed by the Massachusetts Department of Occupational Safety (DOS)
- Supplier Diversity Office (SDO) f/k/a "Massachusetts State Office of Minority and Women Business Assistance Minority Business Assistance" (SOMWBA) Enterprise/Disadvantaged Business Enterprise (MBE/DBE)



- Certified in the Massachusetts Residential Conservation Services (RCS) program and low-income weatherization program through the CSG/CET
- Certified weatherization contractor through the MassSaves program, Department of Housing and Community Development (DHCD Massachusetts), and Weatherization Assistance Program (WAP) through the national utility company grid for services including air sealing and blown cellulose insulation
- Section 3 business through the Boston Housing Authority (BHA)
- Hub Zone Eligible Candidate

Abraham Gonzalez, founder of One Way Development, serves on the Board of Directors for the Massachusetts Minority Contractors Association.

2.2.3 Key Personnel

On every project, One Way Development assembles an on-site construction team complemented with Quality Control, Safety Management, and Cost Estimating support services. During construction, the on-site team will address technical issues, design discrepancies, site latent conditions, etc. Should additional resources be required, the construction team has access to corporate resources to address any equipment, personnel or construction issues that arise. OWD President, Abraham Gonzalez, leads our team.

Key Personnel:

President - Abraham Gonzalez is the founder of One Way Development Inc. and serves on the Board of Directors for the Massachusetts Minority Contractors Association. One Way Development, Inc. was founded in 2004 to specialize in a broad range of construction services. Prior to founding the company, Mr. Gonzalez spent over 10 years in the construction industry and had managed all aspects of running a successful small business. Immediately prior to founding OWD, Mr. Gonzalez served as Project Manager and Estimator for Guillo's Home Improvement where he gained a wealth of experience in all aspects of the construction industry and is now applying that experience to grow and guide One Way Development. Mr. Gonzalez's experience with large companies was that they forgo customer service to focus on their own bottom line. Mr. Gonzalez wanted OWD's edge over its competition to be customer service and has made this the company's highest goal.



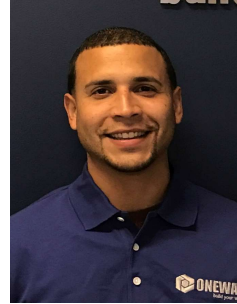
Executive PM - Sumul Shah is the fifth generation of his family to be in the construction business. After earning a BS in Civil Engineering from Brown University and a MS in Construction Management from the Massachusetts Institute of Technology, he worked for the Massachusetts Highway Department, managing more than \$190 million of construction related to the opening of the Ted Williams Tunnel and Central Artery Project. Prior to joining OWD, Sumul formed Lumus Construction to focus on historic restoration, renovation, military, and other complex projects and formed Solaya Energy to focus on renewable energy projects. In 2009, President Obama awarded Sumul the "Small Business Person of the Year" for Massachusetts and 1st Runner Up nationally.



Project Manager - Kristen McNeil has over 20 years experience in construction and project management. Kristen began her career working on road and bridge construction projects throughout Massachusetts and New Hampshire. She then transitioned into building construction, working on Large/ complex projects for the Department of Defense, Department of Homeland Security, and the National Park Service. She currently specializes in projects of all types with the Federal Government. Kristen is a graduate of the University of Massachusetts, Amherst, and is experienced in Construction Scheduling and Safety Management. She is Trained in Army COE QC for Contractors and Red Cross First Aid.



Electrical PM/Super - Alexandro Gonzalez, has over 10 years' experience in commercial, residential, and government construction and 5+ years in supervisory responsibilities. Alex is a licensed Master electrician by trade, and leads OWDs team of electricians. In addition to his Electrical trade qualifications, he has also obtained masterful knowledge in the mechanical trades and is licensed in Refrigeration. Alex is EPA Energy Star Certified.



Project Superintendents:

Our Senior Project Superintendents/Safety/QC personnel that may be assigned to this project include: Allan Bartlett, Rosemberg Santos, Rodrigo Dias, and Renato Dias John Tourkantonis and Terrence Young. Resumes will be provided upon request.

MASS CONSTRUCTION & MANAGEMENT INC.

COMPANY PORFORLIO



MBE and SLBE
Certified

7 Cabot Place, Suite 2
Stoughton, MA 02072

781.886.6901 (P)
781.436-5713 F)

Qualifications Summary

1. Mass Construction & Management, Inc., incorporated in 2007, has been delivering general contracting, preconstruction services, construction management, and property management throughout the Commonwealth of Massachusetts. We are licensed to service and manage both residential and commercial projects, developing retail, public housing, educational facilities, and financial Institutions.

Mass Construction & Management Inc., is a 100% Minority-Owned and operated company with a total of sixteen employees. We are certified by the Massachusetts Supplier Diversity Office, formerly known as the State Office of Minority/Woman's Business Enterprise (SOMWBA).

Mass Construction & Management Inc., total annual revenues for the last 5 years:

2015: \$5,278,213

2016: \$7,948,152

2017: \$8,100,873

2018: \$7,900,000

2019: \$6,190,000

- *MCMI is MBE and SLBE certified.*
- *MCMI is able to self- perform the following trades: Drywall, painting, framing and flooring*
- *We utilized the Massachusetts Minority Contractors Association (MMCA) to hire MBE/WBE/SLBE subcontractors. Our minority workforce percentage on average is 70-80%.*

Mass Construction & Management Personnel Profile

Maxime Charles



Education:

B.Sc., Construction Management 1994
Wentworth Institute, Boston, Mass

A.S. Building Construction Technologies 1994
Wentworth Institute, Boston, Mass

Business Planning, Certificate (MHFA-Sponsored) 1994
UMASS, Boston, Mass

Technical & Licenses:

General Contractor License, Massachusetts
General Contractor License, State of Florida

Senior Construction Management Consultant and President of Mass Construction & Management has over twenty- years of experience in all phases of the construction industry. This experience includes serving as an administrator, construction manager, project supervisor, and estimator. Mr. Charles duties with this construction management and consulting firm are to oversee the construction management procedure provided by the firm and serve as the owner's/client's agent. He is responsible for the overall construction management and delivery of services required by the project. This responsibility includes cost and schedule control and construction consulting assistance throughout the design and construction phase.

In addition to construction management duties, Mr. Charles provides technical and management services, which include General Management, Financial Management Capitalization, Project Management Services, Organizational Management, Planning and Scheduling, Cost estimates.

WORK EXPERIENCE

2007-Present Mass Construction & Management, Inc. - Randolph, MA

- Managed the day-to-day operations of commercial construction projects.
- Negotiate Subcontract agreements and maintained project documentation.
- Review contract drawings, blue prints, and specifications.
- Perform site inspections and resolved issues in a timely manner.
- Coordinated and scheduled subcontractors and project supplies.
- Troubleshoot issues and ensured timely resolutions.

2002-2007-CHARLES GENERAL CONTRACTORS, INC.

Project Manager, Commercial and Residential Construction

Responsible for administration of multiple building construction projects in the Waltham and Boston areas with budgets ranging from \$800,000 to \$1,000,000.

- Prepare budgets, estimates, bids, proposals, and change orders.
 - Performed site inspections and resolved issues in a timely manner.
 - As Safety/EEO officer, insured projects met local, state, and federal regulations.
 - Prepared and presented weekly and quarterly project status reports
- Scheduled and participated in project meetings

2000-2002 - Assistant Vice President/Manager, Facilities Management

STATE STREET CORPORATION, Boston, Mass.

Project Manager for all infrastructure projects at State Street Bank

- Generated estimates for fast track projects.
- Reviewed bids and made final recommendation for award.
- Monitored programs by conducting quarterly on-site visitation and reviews.
- Reviewed monthly invoices and executed quarterly and final analysis reports.
- Supervised the activities of five full-time project associates.

Asset Manager/Construction Specialist, Assets Management Department

1999-2000 BOSTON CAPITAL COPORTATION, BOSTON, MA

Oversaw Design and Construction activities in all 50 States of the United States

- Approved requisitions for payment.
- Performed site expectations prior to receiving tax credits.
- Conducted due diligence and made recommendations to senior management.
- Reviewed specifications and suggested improvements to plans, methods and materials.

Project Listings and Description

1. **Owner:** **Mr. Salnave Blemur**
Project Address: 1280 Blue Hills Avenue, Mattapan, MA
Project Price: \$1,000,000
Project Cost: \$1,000,000
Contact: Charles Bradley, Arch Professional Group

Scope of work: Build a 3600 square foot brick Laundromat. Interior finishes, drywall, painting.

2. **Owner:** **Codman Square NDC**
Project Address: 35 Nightingale Street, Dorchester, MA 02121
59 Spencer Street, Dorchester, MA 02121
Project Price: \$675,000
Project Cost: \$675,000
Architects: Residential Inspection Company
Contact: Dana McQuillin. (617 825-4224) ext. 135

Scope of work – Total gut rehabilitation, interior framing, structural framing, blocking, drywall, plaster, paining, cabinet and flooring, new plumbing, electric service, new wiring, new roof.

3. **Owner:** **Brockton Housing Authority**
Project Address: 157 Tribou Street, Brockton, MA 02301
Project Price: \$350,000
Project Cost: \$350,000
Architect: None
Contact: Frank Hinds (508) 427-9111

4. Owner: **JP Scattered site LLC, Jamaica Plain Neighborhood Development**
JPNDC

Project Address: 76-78 Spring Park Ave, Jamaica plain, MA
3 Buckley Ave, Jamaica Plain, MA
69 Walden Street, Jamaica Plain, MA
106-108 Chestnut Ave, Jamaica Plain, MA
Project Price: \$1,850,000
Project Cost: \$1,850,000
Architect: Elton + Hampton Architect



www.blueatlanticfab.com

Blue Atlantic is a small family owned firm located directly on the water in East Boston, MA. Although this company is fairly new with our inception in 2018. Our ownership group in the Nolan Family has a long successful history working on Boston Harbor. Many of our employees have over 25 years fabricating steel right here in East Boston.

Blue Atlantic Fabrication is a diverse job shop utilizing our direct waterfront access port to Boston Harbor. We have great advantage over similar fabrication firms that must ship overland in smaller divisible loads saving project time and cost. We can build very large structures with minimal transportation costs.

Our physical location in historic East Boston Harbor Shipyard is approximately 40,000 square feet of fabrication space on a 20 acre multi-use industrial facility and marina. We have heavy overhead crane capacity, automated fabrication and welding technology, as well as coating preparation and application.

In over 25 years this facility has produced well over 75,000 square feet of floating docks in and around Boston Harbor, Cape Cod, Upper Massachusetts, and to New York City. It really is our specialty and we are good at it. Our dock sizes can range as large as 30' X 250' to as small as 8'X10' and everything in between.

In addition to dock fabrication we also specialize in other heavy marine structures, bridge construction, buildings, miscellaneous metals, gangways, and non-ferrous alloy metals. We have recently completed our AISC certification and employ many certified welders, coating applicators, and inspectors.

We currently employ 20 hard working technicians, welders, fitters, and operators. We work closely with the local trade schools and councils to help develop and expand our workforce.



256 Marginal St.
East Boston, MA 02128
617.874.8503
info@blueatlanticfab.com

COMPANY PROFILE



Samiotes Consultants, Inc. (Samiotes) is a Civil Engineering and Land Surveying firm founded in 1987 and located in historic Saxonville, (Framingham) Massachusetts. Samiotes has been a certified Woman-owned Business Enterprise (WBE) and a Disadvantaged Business Enterprise (DBE) in the Commonwealth of Massachusetts for Civil Engineering and Land Surveying since 2012.

Samiotes' work experience is diverse, ranging from sophisticated land use feasibility assessments to state-of-the-art site infrastructure designs. Our end users include public spaces and institutions, libraries, public and private secondary schools, colleges and universities, non-profit institutions, industrial facilities, athletic and recreational facilities, hospitals and healthcare facilities. The design philosophy at Samiotes Consultants is to integrate our client's vision with economic viability such that the final product is a sustainable environment serving the needs of the end user and the community in which the project is located.

Samiotes has worked on many recreational and athletic projects including: McNally Park for the City of Medford; the reconstruction and rehabilitation of Legion Field for the Town of Weymouth; the new recreational facilities for the Town of Dedham on the Town-owned Striar property; The Bass Rocks Golf Club in Gloucester; the development of the East Boston Greenway, providing soil testing and the subsequent design of sanitary, drainage, and water services for the 12 acre, 3.3-mile long park; Leo J. Martin Memorial Golf Clubhouse in Weston, MA; the Jim Rice Field at Ramsey Park in Roxbury, a Fenway Park replica that was part of Major League Baseball's RBI program; the Waldstein Playground and Warren Field in Brookline; McKenna Playground and Piety Corner in Waltham; and multiple projects over a ten-year period for Mass Audubon at their 232-acre Drumlin Farm location in Lincoln including the New England Wildlife Exhibit, the Discover Center, relocation of the preschool, and a floating dock system used by staff and children in educational programs.

Samiotes has completed many projects for the Boston Parks and Recreation Department including, but not limited to: the renovation and design of the Frog Pond skating rink and Tadpole Tot Lot on historic Boston Common; Ringer Park and Hardiman Park in Brighton; the William F. Joyce playground in Brighton; Waldren Park and Grenada Park in Jamaica Plain; Chery Playground and the Hunt-Almont Park in Mattapan; the Cronin Playground and the Gertrude Howes Playground in Dorchester; and White Stadium and the Walnut Avenue Playlot at Franklin Park in Boston.

Other current projects with Utile: Hotel at 248 Dorchester Avenue in South Boston; the mixed use development at 1065 Cambridge Street in Cambridge; the 121 Main Street Redevelopment in Brockton; the Dudley Public Library; The VietAID housing development on Bowdoin Street in Boston, and many others.

Samiotes maintains that our approach to Civil Engineering issues is unique and unparalleled in the professional community. Samiotes works through site design in creative ways that result in considerable savings of time and cost. We are always looking for new and better approaches when developing a site design, with an eye on the bottom line. It is our creativity and determination that truly sets us apart.

Samiotes Consultants, Inc.
Civil Engineers + Land Surveyors

20 A Street
Framingham, MA 01701-4102

T 508.877.6688
F 508.877.8349

www.samiotes.com

RESUME

Mr. Garvin is President of Samiotes Consultants, Inc. and has been with the Company since 1993. He is a graduate of the University of Massachusetts Dartmouth with a Bachelor of Science in Civil Engineering. He is a Registered Professional Civil Engineer in the Commonwealth of Massachusetts, the State of Connecticut, the State of New Hampshire, the State of Maine and the State of Rhode Island. Additionally, he is a LEED Accredited Professional.

Stephen has been responsible for several resiliency / sustainable projects throughout the Commonwealth including; the Boston Redevelopment Authority (BRA, now BPDA) E+ Housing Challenge with Utile; the E+ expansion (and original LEED Platinum project) for Artists for Humanity in South Boston; the Alewife Brook Parkway Retail Center in Cambridge, which received the GoGreen Award from the City of Cambridge for its innovative stormwater design; the affordable Hyde-Blakemore Condominiums in Roslindale for Urban Edge with Utile, which incorporated photovoltaic panels on the buildings' roofs and rain gardens for stormwater management; the new Winthrop Middle / Senior High School – which was part of the 2018 Greenbuild tour for resiliency.

Additionally, some projects in Boston include: the historic 48 Boylston Street and subsequent / current 41 Lagrange Street development for the Planning Office of Urban Affairs in Boston Proper, The Frog Pond renovation located on the historic Boston Common; the new Honan-Allston branch, Dudley branch, and Jamaica Plain branch of the Boston Public Library with Utile.

Stephen is currently working on the 248 Dorchester Ave development in South Boston with Utile, the new dock on the Concord River for the Fenn School, Flaherty Park in South Boston, the new Arlington High School, The PAN Athletic Center at Phillips Academy Andover, the Nashua Library renovation in Nashua, NH; the Grand Street Housing Development in Worcester, the River Street development for the Planning Office of Urban Affairs in Mattapan, and several others.

Mr. Garvin's responsibilities on these projects and many others entails obtaining state and local approvals, hydrologic design and calculations, reviewing contract documents and specifications, construction administration, coordination of design issues with various disciplines, and all aspects of site and utility design. In addition to these duties, Mr. Garvin is responsible for the day-to-day management of the firm and Client relationship / development.

In addition to his work at Samiotes, Mr. Garvin is also Chairman of the Town of Sudbury Planning Board.

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Civil Engineers + Land Surveyors

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Framingham, MA 01701-4102

T 508.877.6688
F 508.877.8349

www.samiotes.com

Wojciech Jeffrey Pilat

Project Engineer

RESUME

Mr. Pilat is a Project Engineer for Samiotes Consultants, Inc and has been with the company for 7 years. He graduated from Wentworth Institute of Technology in Boston, Massachusetts with a Bachelor of Science in Civil Engineering.

Wojciech is currently or has worked on the new Saugus Middle / High School; Bajko Rink in Hyde Park; Flaherty Park in South Boston; the Viet Aid Housing Development in Boston with Utile; the Cabot Estate Condominiums in Jamaica Plain; the new track and field at Camp Edwards army base; the new multipurpose arena for Bentley University in Waltham; Green Acres Development in Fitchburg; The Lancaster Campus(DCMM); Dwight Street In Springfield(DCMM); the River Street development for the Planning Office of Urban Affairs in Mattapan; The Belmont Department of Public Works and Police Department; the Goldfarb Library improvements at Brandeis University in Waltham; The Bridge Boston Charter School in Roxbury; the German International School in Boston; the new Dexter Elementary School in Orange, MA; and the Downeast Cider House Brewery in East Boston.

Mr. Pilat's responsibilities on these projects and others include all phases of site planning and design, obtaining local approvals before agencies, various engineering calculations and designs, construction administration and developing site plans and profiles on Civil 3D software.

Prior to joining Samiotes, Mr. Pilat was employed by BSI in Boston, MA as an Engineer for Civil and Subsurface Utility Engineering. He has worked on numerous projects for airport and roadway projects.

Mr. Pilat has completed a multitude of subsurface utility engineering projects for numerous State Transportation Departments across the United States including GDOT, SCDOT, Mass DOT, RIDOT and VTRANS just to name a few. For many of these projects Wojciech lead surveying services, traffic management for excavation, excavation supervision, drawing drafting and quality control. Roadway projects include RIDOT drainage improvements, VDOT highway upgrade projects, GDOT utility conflict matrices.

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Framingham, MA 01701-4102

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F 508.877.8349

www.samiotes.com



THE COMMONWEALTH OF MASSACHUSETTS
Executive Office for Administration and Finance
OPERATIONAL SERVICES DIVISION
One Ashburton Place, Suite 1017
Boston, MA 02108-1552

Charles D. Baker
Governor

Karyn E. Polito
Lieutenant Governor

Michael J. Heffernan
Secretary

Gary J. Lambert
Assistant Secretary for
Operational Services

March 7, 2018

Ms. Despina Samiotes
Samiotes Consultants, Inc.
20 A Street
Framingham, MA 01701-4163

Dear Ms. Samiotes:

Congratulations! Your firm has been renewed as a woman business enterprise (WBE) with the Supplier Diversity Office ("SDO") under the business description of CIVIL ENGINEERING/SUPPORT SERVICES AND LAND SURVEYING SERVICES. Your firm will be listed in the SDO Certified Business Directory and the Massachusetts Central Register under this description. **This letter serves as the sole proof of your SDO certification.** Your designation as a WBE is valid for three (3) years unless revoked pursuant to 425 CMR 2.00.

Your firm's next renewal date is March 29, 2021. SDO will send written renewal notices to your business and/or e-mail address on file approximately thirty (30) business days prior to your firm's three (3) years certification anniversary. Additionally, every six (6) years, certified companies that wish to remain certified may undergo a substantive review which will require certain updated supporting documentation.

SDO also reserves the right to monitor your firm and to perform random spot checks to ensure the firm continues to meet the certification criteria. Your firm is required to notify the SDO in writing of any material changes. Examples include but are not limited to changes in its business description, as well as business phone number, fax number, business' physical location, webpage and e-mail addresses. Other reportable changes include business structure, ownership (the business is sold or transferred), control and outside employment. You also have a duty to report decertification and debarment notices from this or any other jurisdiction. Failure to abide by the continuing duty requirements shall constitute grounds for the firm's decertification.



We look forward to working with you and your firm to maximize its business opportunities. Should you have any questions, please feel free to contact us via email at wsdo@state.ma.us.

Sincerely,

A handwritten signature in blue ink that reads "William M. McAvoy". The signature is written in a cursive style and is positioned above the printed name and title.

William M. McAvoy
Deputy Assistant Secretary and
Chief Legal Counsel



Firm Profile

Introduction

RSE Associates, Inc. was established in 2001 and has since expanded into a multi-disciplinary international firm. With offices across the United States and in London, RSE has successfully completed award winning projects throughout America, Europe and Asia. Our principals and engineers have extensive and diverse experience in complex and demanding projects. We provide the technical ability of a large international firm with the personal service of a small firm.

Technology/BIM

Engineering design is carried out using state-of-the art computer software. We have extensive experience in delivering projects with Building Information Modeling (BIM) requirements, fully integrating the building structure with the overall building information models.

Services

We provide a full range of structural engineering services including:

- New Construction
- Renovation/Adaptive Reuse and Historic Restoration
- Master Plan and Feasibility Study
- Structural Investigation
- Design Competition
- Temporary Construction Support Systems

Building Types

We have extensive experience in a wide range of project types including:

- Museum
- Office
- Retail
- Residential
- High Rise
- Mixed Use
- Educational
- Civic Buildings and Courthouses
- Historic Restoration
- Science and Technology
- Colleges and Universities
- Performing Arts
- Sports Facilities

Design Philosophy

We strive to provide structural designs that are sensitive to owner requirements, architecture, building services as well as cost effectiveness and ease of construction. Design innovations will be balanced with practical considerations. We are committed to delivering quality and personal service.



Bowdoin College Walker Art Museum



Integrated Science Complex, UMass Boston



Cincinnati River Front Park



Lulu Island, Abu Dhabi



Hamilton College Performing Arts Center

Education and Training

- 1999 B.S. (with distinction), Aerospace Engineering – Astronautics Track, United States Naval Academy
- 1999 Minor, Spanish, United States Naval Academy
- 2008 M. Eng., Mechanical Engineering – Energy and the Environment Core, University of Maryland College Park
- 2000 Naval Flight Officer Designation, United States Navy Flight School
- 2006 United States Naval Test Pilot School Graduate
- 2010 Certified Passive House Consultant, International Passive House Association & Passive House Institute U.S.

Work Experience

CEO/Principal Engineer

December 2015 - Present

Ripcord Engineering

Portland, ME

- High-Efficiency Commercial HVAC and Plumbing engineering design
- Mechanical System Commissioning
- Energy Modeling

Commercial Building Services Division Lead

February 2013 – December 2015

Apex Engineering

Falmouth, ME

- Commercial and Industrial Mechanical Engineering
 - High-Efficiency Commercial HVAC and Plumbing system design, Commissioning, Energy Modeling
 - Biomass, natural gas, and renewable fuel oil boiler conversion
 - Campus district heating systems

Mechanical Engineer-in-Training

November 2011-December 2012

Colby Company Engineering

Portland, ME

- Design Engineering
 - Commercial and Industrial HVAC and Exhaust Systems
 - Industrial, Commercial, and Residential Energy Analysis
- Project & Client Management

Advanced Tactics and Flight Instructor

January 2009-November 2011

Electronic Attack Squadron One Two Nine

Whidbey Island Naval Air Station, WA

- Instructed Naval Aviators (pilots) and Naval Flight Officers (backseaters) in airborne electronic attack (exploitation, denial, and destruction of enemy air defenses), aerial dogfighting, and air-to-air missile employment.
- Developed weapon system training curricula and employment tactics for the U.S. Navy's newest aircraft.

Lead Mission Systems Project Officer for the Developmental Test of the EA-18G

June 2006-December 2008

Test and Evaluation Squadron Two Three

Patuxent River Naval Air Station, MD

- Led a multi-site team of over 150 military, government, and contractor personnel in the developmental testing of the EA-18G Growler mission systems, from the delivery of the first prototype to its introduction to the Fleet.
- Flew over 100 hours of test flights that evaluated the EA-18G weapons systems, user interfaces, carrier suitability, structural loads, and flight performance.

EA-6B Electronic Countermeasures Officer (ECMO)

January 2002-January 2005

Electronic Attack Squadron One Three Six

USS Kittyhawk / Atsugi Air Base, Japan

- Fully tactically qualified aircrew in the EA-6B Prowler electronic attack aircraft.
- Aviation/Armament Division Officer in charge of over fifty enlisted sailors.
- Flew combat missions in Iraq in support of Operation Southern Watch and Operation Iraqi Freedom.

References Available Upon Request

Education and Training

- 1998 B.S. (with distinction), Aerospace Engineering – Aeronautics Track, United States Naval Academy
- 1998 Professional Master of Engineering, Aerospace Engineering – University of Maryland College Park
- 2001 Naval Aviator Designation, United States Navy Flight School
- 2007 United States Naval Test Pilot School Graduate

Work Experience

COO/Engineer-in-Training

December 2015 - Present

Ripcord Engineering

Portland, ME

- High-Efficiency Commercial HVAC and Plumbing engineering design
- Mechanical Systems Commissioning
- Energy Modeling
- Advanced IT implementation

Proprietor

March 2012 - December 2015

Idea Werkz

Portland, ME

- Mechanical Engineering for residential applications
 - High-Efficiency residential HVAC and Plumbing system design, Energy Modeling, Construction, and Commissioning
- Mechanical Engineering consultant for industrial processes
 - Analysis of industrial processes for incorporation of efficiency measures
 - Mechanical design of paint and blast facilities
- Automotive Engineering for powertrain retrofits
 - Incorporation of modern advanced powertrain technology into classic automobiles

Department Head

2009 - November 2011

US Navy Electronic Attack Squadron One Three Three

Naval Air Station Whidbey Island, WA

- Operations Officer
 - Developed long term plans and managed the daily operations for an EA-6B squadron of four aircraft, 23 aircrew, and 150 enlisted personnel with an annual operating budget of approximately \$5 million.
 - Transitioned the squadron from a shore-based to a carrier-based unit via an extensive research effort in addition to transition plan development and execution using industry best practices coupled with Naval specific doctrine.
- Safety Officer
 - Administered an award winning comprehensive aviation safety program that resulted in zero flight related mishaps, zero ground mishaps, and zero foreign object damage incidents.
 - Delivered periodic instruction and demonstration of Operational Risk Management (ORM) principles.
- Combat Aviator – land and sea
 - Operation New Dawn - Iraq
 - Operation Iraqi Freedom
 - Operation Enduring Freedom - Afghanistan

Naval Test Pilot

2007-2009

US Navy Strike Aircraft Test and Evaluation Squadron Two Three

Naval Air Station Patuxent River, MD

- Planned, coordinated, and conducted engineering and experimental flight testing on fighter and attack aircraft weapons systems and components.
- Prepared comprehensive test reports to support procurement and/or improvement decisions by the US Federal Government.
- Supervised and executed high-risk field arrested landing tests to evaluate aircraft performance at absolute arrested landing limits.



Charles D. Baker
Governor

Karyn E. Polito
Lieutenant Governor

Michael J. Heffernan
Secretary

Gary J. Lambert
Assistant Secretary for
Operational Services

May 25, 2018

Mr. Richmond So
RSE Associates, Inc.
63 Pleasant Street, Suite 300
Watertown, MA 02472

Dear Mr. So:

Congratulations! Your firm has been renewed as a minority business enterprise (MBE) with the Supplier Diversity Office ("SDO") under the business description of STRUCTURAL ENGINEERING DESIGN AND CONSTRUCTION ADMINISTRATION OF BUILDINGS; GEOTECHNICAL SERVICES INCLUDING FEASIBILITY STUDIES, SITE INVESTIGATION, FOUNDATION DESIGN FOR PERMANENT AND TEMPORARY STRUCTURES, CONSTRUCTION MONITORING AND DOCUMENTATION, GEOTECHNICAL FORENSIC ENGINEERING, AND FAILURE ANALYSIS AND GEOTECHNICAL FIELD INSTRUMENTATION. Your firm will be listed in the SDO Certified Business Directory and the Massachusetts Central Register under this description. **This letter serves as the sole proof of your SDO certification.** Your designation as a MBE is valid for three (3) years unless revoked pursuant to 425 CMR 2.00.

Your firm's next renewal date is June 6, 2021. SDO will send written renewal notices to your business and/or e-mail address on file approximately thirty (30) business days prior to your firm's three (3) years certification anniversary. Additionally, every six (6) years, certified companies that wish to remain certified may undergo a substantive review which will require certain updated supporting documentation.

SDO also reserves the right to monitor your firm and to perform random spot checks to ensure the firm continues to meet the certification criteria. Your firm is required to notify the SDO in writing of any material changes. Examples include but are not limited to changes in its business description, as well as business phone number, fax number, business' physical location, webpage and e-mail addresses. Other reportable changes include business structure, ownership (the business is sold or transferred), control and outside employment. You also have a duty to report decertification and debarment notices from this or any other jurisdiction. Failure to abide by the continuing duty requirements shall constitute grounds for the firm's decertification.



We look forward to working with you and your firm to maximize its business opportunities. Should you have any questions, please feel free to contact us via email at wsdo@state.ma.us.

Sincerely,

A handwritten signature in blue ink that reads "William M. McAvoy". The signature is written in a cursive, flowing style.

William M. McAvoy
Deputy Assistant Secretary and
Chief Legal Counsel





MAKING IMPOSSIBLE, POSSIBLE

Our professional yet out-of-the-box solutions help make the seemingly impossible possible. Founded in 2009, Fraser Engineering Services has grown into a firm that offers comprehensive project planning, design, and implementation services to different industries, both public and private.



What We Do

1. Traffic and transportation engineering
2. General civil engineering design
3. Surveying services
4. Geographic information system applications
5. Feasibility Study

Project Experience

- Northeastern University ISEC Building - Owner's Rep.
- Broadway Village - Traffic Impact Study
- Island Foods, Martha's Vineyard - Feasibility Study
- Winchester Street Traffic Signal Improvement
- Town of Winthrop, Low Strass Bicycle Network Design
- 25 Sixth Street, Civil Site Plan Design

Differentiators

Experienced engineering staff ■ Effectively delivered engineering services ■ Ability to collaborate with multi-disciplined engineering firms and staff ■ Ability to take projects from initial concept to completion ■ Cost Effectiveness ■ Great Customer Support

Firm Information

- Certified Minority Business Enterprise
- Professional Engineer MA #49843
- Professional Engineer NY #091405
- Duns #078600469
- EIN #26-4290451
- IMSA Work Zone Temporary Traffic Control Technician (Cert #ZZ_101723)
- IMSA Traffic Signal Technician Level I (Cert #AA_101723)
- OSHA 30 Hour Certification

Staff & Network Include:

- Registered Professional Engineers, Land Surveyors and Architects
- Professional Traffic Operations Engineers (PTOE)
- PHD and M.S in Civil Engineering



GENERAL CIVIL DESIGN, TRAFFIC, TRANSPORTATION, AND SURVEYING ENGINEERING SERVICES



Kurt A. Fraser P.E. (President/CEO)

EDUCATION

Master of Science Civil Engineering (Northeastern University)
Certificate in Construction Project Management (Northeastern University)
B.S. in Civil Engineering (Northeastern University)

LICENSURE

P.E. License - State of Massachusetts
P.E. License - State of New York

CERTIFICATIONS

MBE – Certified with Operational Service Division
IMSA - Traffic Signal Field Technician Level II
IMSA Work Zone Temporary Traffic Control Technician

PROFESSIONAL AFFILIATIONS

Institute of Transportation Engineers
Massachusetts Institute of Transportation Engineers
American Society of Civil Engineers
Boston Society of Civil Engineers
Conference of Minority Transportation Officials
National Society of Black Engineers

PROFESSIONAL EXPERIENCE

Mr. Fraser has over 25 Years of experience in the Civil and Transportation Engineer Field, as a public sector employee, and the owner of his own civil engineering firm, Fraser Poly Engineering Services.

His areas of expertise include design and management of building construction projects. Roadway, and traffic engineering designs utilizing such design software as: Vissim, Synchro, AutoCAD Civil 3D, Auto Turn, Hydro Cad, Primavera Project Planner.

He has thorough knowledge of the calculations and analysis involved in the design of these projects, coupled with deep construction field experience including oversight of building, roadway, and signalization projects.

He has served on the Transportation Board for the Town of Brookline where he communicated transportation related projects to planning board members, citizen groups, and other transportation professionals.



THE COMMONWEALTH OF MASSACHUSETTS
Executive Office for Administration and Finance
OPERATIONAL SERVICES DIVISION

One Ashburton Place, Suit 1017
Boston, MA 02108-1552

Charles D. Baker
Governor

Karyn E. Polito

Lieutenant Governor

Michael J. Heffernan

Secretary

Gary J. Lambert

Assistant Secretary for Operational Services Division

February 12, 2020

Mr. Kurt A. Fraser

Fraser Poly-Engineering Services

35 Norman Street

Milton, MA 02186

Dear Mr. Fraser:

Congratulations! Your firm has been renewed as a minority business enterprise (MBE) with the Supplier Diversity Office ('SDO') under the business description of **ENGINEERING CONSULTING FIRM SPECIALIZING IN CIVIL ENGINEERING, TRAFFIC ENGINEERING, TRANSPORTATION PLANNING AND ROADWAY AND HIGHWAY DESIGN AND CONSTRUCTION PROJECT MANAGEMENT AND SURVEYING**. Your firm will be listed in the SDO Certified Business Directory and the Massachusetts Central Register under this description. **This letter serves as the sole proof of your SDO certification.** Your designation as a MBE is valid for three (3) years unless revoked pursuant to 425 CMR 2.00.

Your firm's next renewal date is November 23, 2022. SDO will send written renewal notices to your business and/or e-mail address on file approximately thirty (30) business days prior to your firm's three (3) years certification anniversary. Additionally, every six (6) years, certified companies that wish to remain certified may undergo a substantive review which will require certain updated supporting documentation.

SDO also reserves the right to monitor your firm and to perform random spot checks to ensure the firm continues to meet the certification criteria. Your firm is required to notify the SDO in writing of any material changes. Examples include but are not limited to changes in its business description, as well as business phone number, fax number, business' physical location, webpage and e-mail addresses. Other reportable changes include business structure, ownership (the business is sold or transferred), control and outside employment. You also have a duty to report decertification and debarment notices from this or any other jurisdiction. Failure to abide by the continuing duty requirements shall constitute grounds for the firm's decertification.

We look forward to working with you and your firm to maximize its business opportunities. Should you have any questions, please feel free to contact us via email at wsdo@state.ma.us.

Sincerely,

A handwritten signature in blue ink that reads "William M. McAvoy". The signature is written in a cursive style with a large, stylized 'W' and 'M'.

William M. McAvoy
Deputy Assistant Secretary and
Chief Legal Counsel

KEY PERSONNEL RESUMES

JMS PERSONNEL

JMS maintains a full time staff of naval architects and engineers with unique qualifications related to oil transportation vessel design and operations. One of the advantages of our engineers and naval architects is their sea-going and vessel operations experience. This provides us with a unique perspective on the needs of our customers beyond pure “engineering”. JMS personnel have the depth and unique qualifications to respond to the needs of our customers on short notice. Further advantages are drawn from our unique combination of high-end analytical engineering expertise with practical seafaring experience. Our staff of degreed naval architects, marine engineers, and registered Professional Engineers has extensive hands-on experience in vessel operations and shipyard construction practices. This unique blend of qualifications allows JMS to deliver rapid and innovative solutions to complex problems. All of our engineers have considerable experience with regulations and standards of classification societies including especially those of ABS, the U.S. Coast Guard, UNOLS, and SOLAS. Our ability to relate to vessel crews, shipyard laborers, as well as technical representatives from MARAD, U.S. Coast Guard, UNOLS, and ABS, and other government agencies is an important aspect of providing the most comprehensive service.

KEY PERSONNEL

JMS proposes the following assignment of Key Personnel to lead the staffing requirements of the project. The key personnel will be the primary points of contact for this project but will be supported by a full time staff of naval architects and specialists.

Name	Project Role
T. Blake Powell	Principal Engineer / Naval Architect / Marine Engineer
William J. Foster, P.E.	Project Mgr. / Senior Naval Architect / Marine Eng. / P.E. Review / Quality Control
David Forrest, P.E.	Naval Architect / Marine Engineer / P.E. Review

SUBCONTRACTORS

JMS can perform all the tasks of this project completely with in-house personnel and does not intend to use any subcontractors for this project. However, JMS does maintain an active network of prequalified, small business subcontractors to augment the production capabilities and expertise of our in-house staff of professional engineers, naval architects, marine, electrical, and mechanical engineers. This network has been established to provide a surge capability, should it be needed. Whenever subcontractors are needed, tasks are still completely managed in-house by JMS personnel. All subcontracted companies and individual consultants must meet the Quality Control requirements of our ISO 9000 Quality Management System (QSM) before their services can be utilized by JMS.

QUALITY CONTROL, GSA SCHEDULE, VETERAN-OWNED SMALL BUSINESS

JMS takes very seriously the critical link between project management and the quality of deliverables. JMS maintains a formal project planning and management system that is integrally linked to our ISO 9000 Quality Management System (QMS). Our QMS ensures a consistent and thorough approach throughout every design project, regardless of the project size. JMS takes pride in not only being able to offer well-managed projects that produce consistent, high quality, and professional deliverables, but to do so at competitive rates. JMS is a GSA contract holder offering all of our services at Government-approved rates. JMS is also a Veteran-Owned Small Business since 1988.

JMS CAPABILITIES AND QUALIFICATIONS SUMMARY

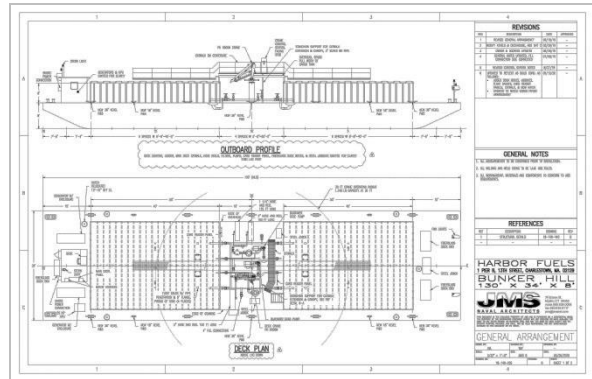
JMS Naval Architects is a specialty maritime engineering firm providing naval architecture, marine engineering, marine surveying, and marine casualty response services to assist ship owners in the management of their fleet. With a unique combination of high-end analytical engineering expertise and practical deck plate experience, JMS delivers innovative and cost-effective solutions to complex problems. JMS is committed to delivering quality products with highly personalized and responsive service. JMS' offices are located along the Mystic River in downtown Mystic, CT. The historic community is rich in maritime tradition and offers an ideal setting for a maritime business.

NAVAL ARCHITECTURE & MARINE ENGINEERING

JMS has completed a wide range of naval architecture and marine engineering projects for commercial and government clients. JMS engineers provide technical consulting and marine design assistance for new design, repairs, modifications, and installation of shipboard systems for customers representing all sectors of the maritime industry.

JMS naval architects combine operational experience and technical expertise to deliver innovative, practical and cost-effective solutions for traditional naval architecture services including:

- Stability Analysis and Inclining Tests
- Structural and Finite Element Analysis
- Engineering Feasibility Studies
- Preliminary and Concept Design
- Computer Aided Design and Engineering
- Vessel Structural Repair and Modification
- Shipyard Construction Technical Support
- Marine Engineering
- Repowering Design and Specification
- Dry Docking and Launching Calculations
- Heavy Lift Engineering
- Regulatory Plan and Drawing Review
- Computerized Loading Programs
- Expert Witness Testimony



Our workboat design portfolio includes dry docks, barges, research vessels, and special-purpose vessels. JMS supports vessel owners through shipyard qualification and selection, bid review and contract negotiations, on-site construction management and sea trials. JMS also develops studies and analyses to assist vessel owners in the management of their fleet including service life extension studies, long term fleet renewal studies, regulatory impact studies and design feasibility studies.



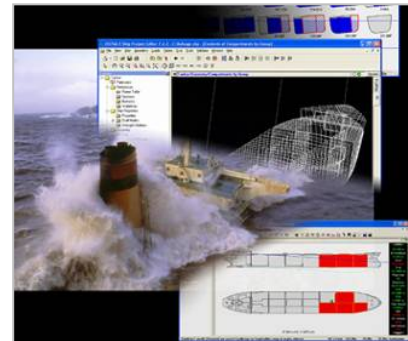
MARINE SURVEYING AND VESSEL OPERATIONS SUPPORT

JMS engineers rely on operational and deck plate experience to develop practical and cost-effective solutions to technical problems. This perspective comes from extensive experience conducting vessel inspections for a wide variety of purposes. Surveys include:

- Condition surveys of ships, barges and dry docks for preacquisition or insurance purposes
- Structural assessments of aging vessels for steel renewal and overhaul planning
- Research vessel assessments of ability to effectively conduct oceanographic research
- Vessel safety inspections
- On-site vessel construction technical oversight
- Sea trials
- Tow surveys and planning
- Safety management system audits
- Accident investigation surveys

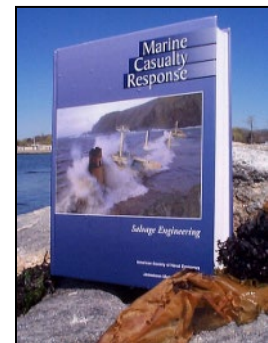
MARINE CASUALTY RESPONSE AND SALVAGE ENGINEERING

JMS has specialized expertise in salvage operations and is a member of the American Salvage Association. JMS' expertise in naval architecture, diving, and vessel operations combine to create unique qualifications related to marine casualty response. Using computer modeling naval architecture software (HECSALV), JMS engineers provide rapid assessments of ship stability and strength for intact or damaged ships. Through the Emergency Response Network, JMS engineers respond 24 hours a day to provide salvage engineering support for OPA 90 compliance to tanker and barge customers. During salvage evolutions JMS will build a consensus between the owner, Incident Commander, Coast Guard, and on-scene authorities.



- On-Site Salvage Engineering Support for Marine Casualty Operations
- Vessel Damage Assessments
- Damaged Strength and Stability Calculations
- Salvage and Lightering Plans
- Tow Plans
- Salvage Engineering Support for OPA 90 Compliance
- Accident Investigation and Forensic Engineering

JMS was selected by the U.S. Supervisor of Salvage to write the *U.S. Navy Salvage Engineers Handbook*. This has become the authoritative text on the subject of salvage engineering. JMS, in cooperation with the American Society of Naval Engineers (ASNE) produced an edition of the U.S. Navy handbook tailored to the commercial marine industry titled, *Marine Casualty Response: Salvage Engineering*. Additionally, JMS was selected by the U.S. Navy in 1990 to write the six volume Salvage Manual including *Strandings*, *Harbor Clearance*, *Firefighting/Damage Control*, *Deep Ocean Recovery*, *Emergency Petroleum Offload*, and *Oil Spill Response*. JMS has also developed an iPhone application based on JMS's *U.S. Navy Salvors Handbook*. This valuable reference provides information on basic naval architecture, salvage operations, rigging, and towing.



JMS Naval Architects

JMS Capabilities and Qualifications

*This proposal contains confidential and privileged information.
Any unauthorized review, use, disclosure, or distribution, in whole or in part, is prohibited.*

ACCIDENT INVESTIGATION AND EXPERT WITNESS PROJECTS

JMS has performed accident investigation and forensic engineering analyses for cases involving submarine collisions, passenger vessel sinkings, vessel groundings, vessel fires and explosions, marine construction barges and vessel design failures. Expert reports, depositions, and litigation support have been provided for the National Transportation Safety Board, US Coast Guard investigations and maritime attorneys.



JMS offers a combination of talents not commonly found in one place for consultants to marine litigation proceedings. In addition to engineering and shipboard operations expertise, JMS' visualization services play an important role creating convincing and technically accurate expert witness testimonies that educate non-engineers or non-seagoing people about basic naval architecture principles and ship operations. In many cases, JMS has created 3-dimensional, technically accurate, computer models of the subject vessel to support the engineering analysis.

FACILITIES AND RESOURCES

JMS offices are located along the Mystic River in downtown Mystic, CT. We believe that living and working in beautiful surroundings is conducive to creativity and innovation. Strategically located between Boston and New York harbors also allows us to serve our customers in two busy maritime ports. The historic community of Mystic, CT is rich in maritime tradition and offers an ideal setting for our growing business. The area is known for its high quality of life, drawing visitors each year for the boating, world class sailing, beaches, cultural attractions, museums, and scenic coastline villages. JMS maintains a full suite of engineering and design software. Engineers are proficient in AutoCAD 3D; Rhino and Autodesk 3DS MAX for 3D modeling; HECSALV and GHS for hydrostatic and structural analysis; ALGOR finite element analysis software; and NavCad for the prediction and analysis of vessel speed and power performance.



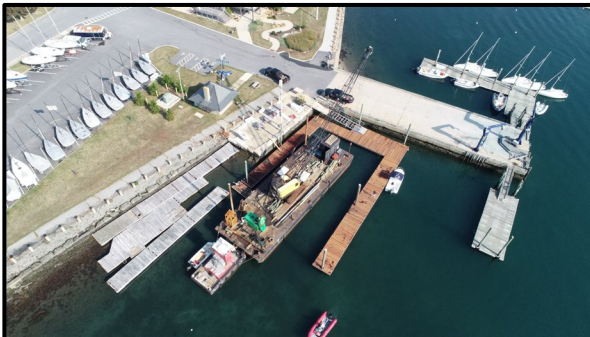
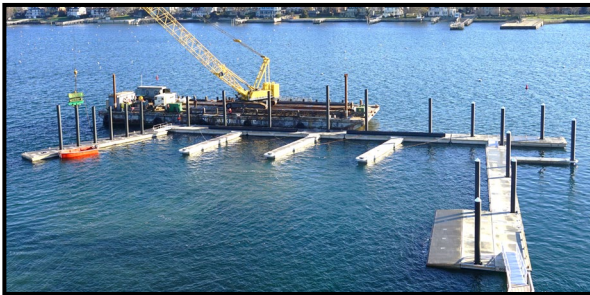
Company Background

Harbor Engineering, LLC is a Rhode Island based engineering firm focused exclusively on planning, permitting and designing waterfront and coastal structures. For over ten (10) years our firm has supported clients in Rhode Island, Massachusetts, Connecticut and New York.

The principal of Harbor Engineering, LLC has his Masters of Engineering degree in Coastal Engineering and has been working in this field for over 25 years. In addition to our expertise in our chosen waterfront & coastal engineering disciplines, we feel it is critical to provide our clients with the highest level of customer service including:

- Educating the client about the planning, regulatory, design and construction process
- Empowering the Client to make decisions that will guide the design effort
- Keeping the Client informed throughout the project
- Being readily available and responsive to the Client

REPRESENTATIVE SERVICES:



Coastal Engineering

- Site exposure (wind, wave, current, flood) assessments
- Breakwater and revetment design
- Littoral Process Studies
- Beach Nourishment

Waterfront Engineering

- Marina planning, reconfiguration and design
- Facility condition assessments
- Managing dive inspections
- Structural analysis of waterfront structures
- Pier design
- Bulkhead and seawall design
- Boat ramp planning & design
- Port facility structures design

Other Services

- Marina Market Assessments
- Hydrographic surveys & Dredge planning
- Revisions to FEMA flood maps and flood regulations consulting



August J. Kreuzkamp, M.Eng., P.E.

Principal

EXPERIENCE SUMMARY

Mr. Kreuzkamp has worked in the field of coastal engineering and waterfront construction since 1992. He has inspected, designed and overseen the construction of a diversity of coastal, waterfront and harbor structures including commercial and residential bulkheads, seawalls, breakwaters, groins, piers, revetments, relieving platforms, marina facilities and port facility structures.

In addition to construction-related services, Mr. Kreuzkamp has prepared littoral processes studies; provided property condition assessments, successfully challenged the flood rating for waterfront properties with FEMA and has extensive experience filing for permits on behalf of clients with Federal, State and local government agencies.

A variety of clientele have benefited from Mr. Kreuzkamp's experience including waterfront property owners, marinas, yacht clubs, municipalities, waterfront associations, engineers, surveyors, marine contractors, attorneys, architects, developers and realtors.

PUBLICATIONS

Pilot Program to Quantify Shoreline Changes in Lee County with R.G. Dean, University of Florida, Gainesville, Florida, February 1997.

Analysis of Conventional Aerial Photography to Determine Shoreline Position, Master Thesis, University of Florida, Gainesville, Florida, May 1997.

AREAS OF SPECIALIZATION

- Facility Condition Assessments (*above and underwater*)
- Shoreline Stabilization
- Rehabilitation of waterfront structures
- Waterfront and Port structural design
- Marina & waterfront planning and design
- FEMA Flood Map Revisions
- Dredge Planning
- Regulatory Permitting

EDUCATION

University of Florida, Master of Engineering, Coastal & Oceanographic Engineering, 1997

Roger Williams College, Bachelor of Science, Civil Engineering, 1992

REGISTRATIONS

Professional Engineer,

Rhode Island, No. 7949

Massachusetts, No. 45573

Connecticut, No. 21425

New York, No. 081837

AFFILIATIONS

American Society of Civil Engineers

Coasts, Oceans, Ports and Rivers Institute

Rhode Island Marine Trades Association

Permanent International Association of

Navigation Congresses (PIANC)

Association of Coastal Engineers



Key Projects:

Sail To Prevail; Newport, RI Responsible for planning, designing and obtaining the regulatory permissions needed for the new waterfront facilities located inside the Sail Newport area of the Fort Adams State Park. The project involved the replacement of the existing floats and piles that have a modified configuration to maximize the facility's footprint and provide wider floats to accommodate the needs of handicap individuals who visit the facility.

Charlestown Marina; Charlestown, MA Responsible for the planning and design of the new marinas on Boston Inner Harbor that were formerly known as Shipyard Quarters. The expanded marinas are located at Piers 6 & 8 in the former Charlestown Navy Yard. Combined, the facilities cover an area of over 20 acres and offer over 450 dedicated slips that are protected by floating wave attenuators. In addition to planning the marina, Mr. Kreuzkamp designed the new harborwalk at Pier 6 and the steel anchor pile systems at both locations that considered deep water, poor soil conditions and large loads including those associated with the floating wave attenuator and vessels that range in size between 25ft and 400ft.

Safe Harbor Marinas - New England Boatworks, Portsmouth, RI. Responsible for planning the dredging of the facility's entrance channel and inner basin. Efforts included the coordination of sampling and testing of the existing material and seeking the regulatory approvals from the State of Rhode Island (RI Coastal Resources Management Council & RI Department of Environmental Management) and US Army Corps of Engineers to remove up to 24,000 cubic yards of material from the facility to accommodate larger (200-ton) deep-draft vessels.

Brown University, Marston Boathouse Providence, RI

Oversaw the design of the new waterfront infrastructure at the University's Crew facility that is used by the Men's and Women's Crew Teams. The updated facilities included a new bulkhead, revetment, pier and series of floats to accommodate the needs of the crew teams and coaching staff. Mr. Kreuzkamp regularly coordinated with various representatives of the University, managed the fieldwork and design of all new infrastructure, and prepared all project plans required for regulatory review and bidding/construction. After the contract was awarded, Mr. Kreuzkamp observed work in progress through the completion of the project; completed on time and under budget.

Sail Newport – North Alofsin Pier Improvements; Newport, RI

Planned the expansion of the existing waterfront including a new section of dockage to support the 2018 Volvo Ocean Race and future water transportation services. Designed the float anchor piles and pier extensions. Prepared plans and forms required for regulatory review and bidding/construction.

Gurney's Newport Resort & Marina; Newport, RI

Planned the initial development of the new marina on Newport Harbor to support a wide range of vessel sizes, including superyachts. Managed the fieldwork including surveys and geotechnical investigation. Performed the site exposure assessment to quantify environmental forces including wind, waves and current. Designed the proposed infrastructure including the float anchor piles, access pier and the entertainment area (pier). Prepared plans required for regulatory review and bidding/construction and observed work in progress through the completion of the project.



Key Projects:

Allen Harbor Boat Ramp; North Kingstown, RI

Planned the expansion of the former 2-lane ramp with a 3-lane ramp that conforms with RIDEM requirements. Prepared all plans needed for regulatory review, bidding and construction. Managed the preparation of the regulatory applications and personnel responsible for observing work in progress through construction.

Barrington Yacht Club; Barrington, RI

Planned the reconfiguration of the 103-boat marina taking into account direction from Club stakeholders, local currents and area bathymetry to create a balanced long-term marina layout. Prepared a detailed site exposure assessment, prepared all regulatory applications and lead the design of the marina anchor piles.

Newport Yacht Club; Newport, RI

Evaluated the condition of a deteriorated timber wave fence, identified temporary repairs and designed its ultimate replacement with a series of concrete attenuator floats to protect the club's marina basin from wave action within Newport Harbor.

Conanicut Marine Services; Jamestown, RI

Worked with the owner to develop a variety of marina layouts that considered local wind/wave exposure and slip demand. As part of the planning process, Mr. Kreuzkamp coordinated field investigations including a hydrographic survey, Subaquatic Vegetation (SAV) surveys and wind/wave site exposure assessment. Conceptual plans prepared were accompanied with conceptual cost estimates for the client's consideration. Since the initial planning effort, Mr. Kreuzkamp has worked with the client to select a preferred attenuator float manufacturer and designed the anchor piling for new sections of the marina.

Fort Point Associates, Inc.; Boston, MA

Collaborated with the client on a variety of projects along the Boston waterfront including Boston Autoport, International Cargo Port, Constitution Center, Thompson Island, Lewis Wharf and Commercial Wharf. Each project required familiarity of the site and engineering services in support of various regulatory filings.

Dwyer / Partners, LLP; Boston, MA

Mr. Kreuzkamp worked with a team of trial attorneys, engineers and construction estimators to defend a multi-million dollar claim stemming from a proposed development on a waterfront parcel that never came to fruition. In addition to providing a technical report full of robust analysis and credible conclusions, which were important to driving a favorable settlement, Mr. Kreuzkamp became an integral team member educating the team about marine engineering issues, preparing them for depositions, and testing/evaluating their theories and approaches as they related to Harbor's areas of expertise.

Boston Yacht Haven; Boston, MA

Assessed the condition of the existing marina infrastructure and developed a variety of conceptual layouts that considered the Site's exposure to commercial wake, wind waves and improved dockage for vessels including mega yachts. Mr. Kreuzkamp worked with the owner to develop the facility over several phases which has evolved into a very popular 100-slip destination marina in downtown Boston.

Suntex Marinas' Liberty Landing Marina; Jersey City, NJ

Responsible for completing a condition assessment for the 550-slip marina to determine the facility's condition, deferred maintenance and limitations prior to the client purchasing Liberty Landing Marina. Mr. Kreuzkamp oversaw various efforts including



Key Projects:

the hydrographic survey and topside & underwater inspection efforts.

Head of the Bay Gateway: Providence, RI –
Oversaw the marina market assessment in support of the redevelopment of the derelict waterfront facility previously known as “Shooters Café Marina”. The effort included interviewing area stakeholders, local marina operators, and professionals involved in the inner workings of the Providence waterfront for gauging how the future development could compliment ongoing improvements to Providence. Conceptual layouts for the future marina were developed considering the anticipated demand, potential user groups, existing shoreline conditions, and water depths. The final report included the results of the marina market assessment, layouts and guidance as to how to move the project forward.

Montauk Yacht Club & Resort: Montauk, NY –
Responsible for working with various representatives of Island Global Yachting to plan the reconfiguration of the existing marina to better accommodate mega yachts with consideration to the local demand for slips based on a limited marina market assessment. Mr. Kreuzkamp was also involved with investigating various structural issues along the property’s shoreline and providing various solutions for the IGY Asset Management group’s consideration.

New England Development, LLC; Cape Cod, MA – Led the waterfront engineering due diligence efforts of a marina in Cape Cod, Massachusetts that included a detailed market assessment of the region to determine the potential slip demand for an expanded facility; site exposure assessment to quantify wind-generated waves which included a series of numerical wave models to forecast wave conditions associated with specific storm

recurrence intervals and to quantify the wave climate within the proposed marina basin.

To assess the condition of the existing 1,000 foot long timber pier structure, Mr. Kreuzkamp worked with a surface-supplied commercial diver equipped with continuous two-way communications, video and pneumofathometer to complete a detailed structural condition assessment of the pier. The final report included the identification of deferred maintenance items and recommendations on how to proceed with the redevelopment including estimated construction costs.

Port of Quebec Marina; Québec, Canada –
Project Manager responsible for the master planning of the Port’s marina facility including a full reconfiguration of its layout. Effort included the completion of the facility’s condition, market assessment to determine the potential slip-demand for a pending marina expansion and the development of potential marina layout configurations.

Astra Waterfront Development; Budva, Montenegro- Mr. Kreuzkamp traveled to the Site to participate in a planning charette that included the developer, planners and architects to evaluate site conditions; and assess the feasibility of constructing a recreational marina facility with coastal amenities along the development’s shoreline. The project considered the development of over 9.4 hectares along the western side of a peninsula situated on the eastern side of Budva Bay (directly across from the 2,500 year old city referred to as the ‘Old Town’ of Budva).

Rodney Bay Marina; Saint Lucia, West Indies
Responsible for the technical review of the contractor’s design submittals including the structural analysis of the dock manufacturer’s floating docks and corresponding anchor pile



Key Projects:

system. In addition, toured the Walcon Marine Ltd manufacturing facilities in Hampshire, England to ensure compliance with the project's performance specifications and approved shop drawings; observe internal QA/QC procedures and confirm the manufacturer's delivery schedule.

Ginn Company; Stamford, CT – Project Manager responsible for the inspections of an existing marina facility that was being considered for acquisition and redevelopment. The inspection report offered an independent overview of the marina facilities and an opinion on the general condition, deferred maintenance issues and other concerns of the facility including its shoreline, piers, floats and wave fence.

Ayla Development Corporation, Aqaba, Jordan
Traveled throughout the region along the Jordanian and Egyptian coasts to investigate other marina facilities as part of a market assessment, assessed the coastal structures planned throughout the project and participated in an architectural charette that involved the planning of a marina that will be part of a future 1,500 unit marina village off the Gulf of Aqaba.

New England Boatworks: Portsmouth, RI – Project Manager responsible for performing a series of initial inspections including geotechnical borings and excavations behind an existing bulkhead to determine soil types in the area and condition of the bulkhead's existing anchorage system. The results of these activities were taken into consideration in order to design a relieving platform needed to support up to a 225 ton capacity mobile crane staged in the vicinity of the facility's sheet pile bulkhead.

Forty 1° North, Newport, RI - Performed a detailed underwater and topside inspection of the marina facility. Based on the results of the

inspection, designed improvements to surgically upgrade specific areas of the facility to ensure its overall serviceability with consideration to cost and schedule for its first season.

In preparation for the second season, Mr. Kreuzkamp was responsible for planning, preparation of permit applications, designing and overseeing the construction of the new marina facility including the bulkhead, piers, seawall improvements, anchor piling, marina reconfiguration and utility requirements considering mega yacht needs.

Ambergris Cay Partnership, British West Indies, Turks & Caicos Islands - Provided consultation services reviewing the design of a rubble mound revetment by others that will protect the extension of an airplane runway and provided the client recommendations including modifications to the proposed structure as well as other structural alternatives that considered availability of materials, constructability and overall cost.

Conusub: Jamestown Verrazzano Bridge, North Kingstown/Jamestown, RI – The Jamestown Verrazzano Bridge is an 8,500 foot long structure which supports State Highway 138 over the West Passage of Narragansett Bay. The bridge is supported by 28 trestle bents and 23 pier structures that vary in size and construction. Mr. Kreuzkamp was the Team Leader for the underwater inspection effort, working with Conusub (client) to perform a routine sub aqueous inspection of RI Bridge #800. The underwater condition assessment included the inspection of all the submerged elements of the bridge structure, scour assessment, managing the tide gauge and developing the report to RI Department of Transportation.



Leadership



SARAH EUSTIS
CEO

Sarah oversees all aspects of strategic direction and development and is the founder and owner of Main Street Hospitality. She is the third generation leader of The Fitzpatrick Family of businesses, based at The Red Lion Inn in Stockbridge, Ma. Sarah brings over 25 years of experience in retail operations, marketing, design and brand development and previously held senior leadership positions at Polo Ralph Lauren, Gap Inc., Limited Brands and Etam in France. Sarah is a graduate of Smith College and lives with her husband and two boys in Great Barrington, Ma.



JACKSON DONOYAN
VP OPERATIONS

Jackson is responsible for achieving key performance metrics at each property by providing strong leadership across a variety of interconnected disciplines, including revenue generation, operational oversight, and talent development. Jackson brings 30 years of operations experience, including NYLO Hotels, Noble House, The Liberty Hotel in Boston, among others.

Leadership



GRETA KIPP
VP PERFORMANCE

Greta is responsible for helping to optimize performance at every level by providing analysis and organization in areas such as business intelligence, pricing/distribution strategies, asset management, financial reporting/budgeting and business planning for all of the Main Street properties. She joined the Main Street team in 2014. Greta is a US Army veteran and following her service and university studies she began a hotel career in leading independent hotels at properties to include Topnotch Resort in Stowe and The Williams Inn at Williams College.



JOE GALLO
STRATEGIC FINANCIAL ADVISOR

In his partnership with Main Street, Joe leads all strategic growth initiatives, development projects and acquisitions, financial relationship and investor development. Joe has more than 30 years of experience advising companies, investors and boards of directors regarding corporate finance, capital raising, investing, M&A, growth strategies and financial operations. Joe spent fourteen years with Citigroup as a Managing Director of Investment Banking.

Portfolio



THE RED LION INN
Stockbridge, MA
Fitzpatrick Family

- \$12.1M Rev
- 51% Occ
- \$125 RevPAR
- 125% RevPAR Index
- Renowned Brand
- \$14mm in capital improvements since 2007



PORCHES INN
North Adams, MA
Berkshire Hills
Development

- \$2.9M Rev
- 62% Occ
- \$157 RevPAR
- 154% RevPAR Index
- Partner with Mass MoCA
- \$15mm historic conversion in 2001



HOTEL ON NORTH
Pittsfield, MA
Tierney Group

- \$3.9M Rev
- 65% Occ
- \$119 RevPAR
- 133% RevPAR Index
- \$15mm Tax Credit Project
- Opened June 2015



BRIARCLIFF MOTEL
Pittsfield, MA
Main Street + Investors

- \$446k Rev
- 51% Occ
- \$69 RevPAR
- 98% RevPAR Index
- \$15mm Tax Credit Project
- Opened October 2017

Portfolio



HAMMETTS HOTEL
Newport, RI
Peregrine Group

- \$2.1M Rev
- 59% Occ
- \$175 RevPAR
- 161% RevPAR Index
- High style/Limited service
- \$30mm New Build



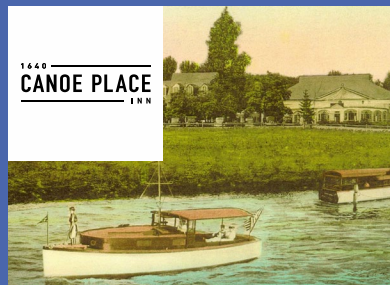
THE BEATRICE
Providence, RI
Paolino Group

- Opening 2021
- \$3m projected stabilized revenue
- F&B 3rd party partner



SEVEN HILLS INN
Lenox, MA
Wardman Group

- \$633k Rev
- 48% Occ
- \$92 RevPAR
- 116% RevPAR Index
- Approachable luxury
- New owner / Renovation 2019



THE CANOE
PLACE INN
Hampton Bays, NY
The Rechler
Equity Group

- Opening 2022
- \$10-12m projected stabilized revenue
- 25 Rooms
- Extensive F&B
- Luxury Destination
- Expansive amenities
- 350p capacity event space

*All results are from 2019 annual, except Hammetts Hotel (only available data is July-Oct 2020 since opening) and Seven Hills Inn (July-Oct 2020, MSH took over in July)